

Thornydale Storage

SPECIFIC PLAN



August 2022

Thornydale Storage Specific Plan

Project Team

ZDC Properties, LLC
18381 Long Lake Drive
Boca Raton, Florida 33496

Lazarus & Silvyn, PC
5983 East Grant Road, Suite 290
Tucson, Arizona 85712

Engineering and Environmental Consultants, Inc.
555 East River Road, Suite 301
Tucson, Arizona 85704

Novak Environmental, Inc.
4574 North First Avenue, Suite 100
Tucson, Arizona 85718

M Esparza Engineering, LLC
2934 West Salvia Drive
Tucson, Arizona 85745



Table of Contents

I.	Introduction & Policy	I-1
A.	Introduction	I-1
B.	Compliance with Pima Prospects	I-3
1.	Use of Land	I-3
II.	Land Use Proposal	II-1
A.	Project Overview	II-1
B.	Permitted Uses.....	II-3
C.	Development Standards	II-3
1.	Site Development	II-3
2.	Landscape Standards	II-3
3.	Vehicular & Bicycle Parking	II-4
4.	Loading.....	II-4
5.	Storage Facility Hours of Operation	II-4
6.	Design Standards	II-5
7.	Buffelgrass Elimination	II-5
D.	Scenic Route.....	II-5
E.	Hydrology	II-5
F.	Transportation and Circulation.....	II-8
1.	Proposed Ingress/Egress and Onsite Vehicular Circulation	II-8
2.	Average Daily Traffic & Concurrency.....	II-8
3.	Distances to Existing Drives/Intersections	II-10
4.	Bicycle & Pedestrian Circulation.....	II-10
5.	Traffic Impact Analysis	II-10
G.	Utility Infrastructure	II-10
1.	Sewer	II-10
2.	Water	II-10
III.	Implementation and Administration.....	III-1
A.	Administration and Interpretation	III-1
B.	Phasing and Procedures for Development Review	III-1
C.	Amendments.....	III-1
1.	Minor.....	III-1

Thornycastle Storage Specific Plan

2. Major.....	III-2
IV. Site Inventory.....	IV-1
A. Land Use.....	IV-1
1. Location/Regional Context	IV-1
2. Existing Land Uses.....	IV-1
3. Existing Zoning	IV-1
4. Existing Easements	IV-3
5. Comprehensive Plan	IV-3
B. Topography & Grading.....	IV-5
1. Topographic Characteristics	IV-5
C. Hydrology	IV-5
1. Offsite Watershed.....	IV-5
2. Onsite Hydrology	IV-5
3. Floodplain	IV-6
D. Biological Resources	IV-8
1. Conservation Lands System.....	IV-8
2. Priority Conservation Area.....	IV-8
3. Habitat Protection/Community Open Space.....	IV-8
E. Transportation	IV-10
1. Existing/Planned Offsite Streets	IV-10
2. Public Transit & Alternate Modes	IV-13
F. Utilities	IV-15
1. Sewer	IV-15
2. Water	IV-15
G. Recreation	IV-15
H. Cultural Resources	IV-18
I. Composite	IV-18
V. Conditions of Approval	V-1
A. Purpose	V-1
B. Board of Supervisors Conditions of Approval.....	V-1
VI. References	VI-1

List of Exhibits

Exhibit I.A: Regional Location Map	I-2
Exhibit II.A: Preliminary Development Plan	II-2
Exhibit II.E: Drainage Concept.....	II-7
Exhibit II.F: Distances to Existing Driveways/Intersections	II-11
Exhibit IV.A.2: Existing Zoning & Land Use	IV-2
Exhibit IV.A.5: Comprehensive Plan Designations	IV-4
Exhibit IV.C: Existing Conditions Hydrology	IV-7
Exhibit IV.D: Biological Map	IV-9
Exhibit IV.E: Existing Transportation Network.....	IV-14
Exhibit IV.F.1: Existing PCRWRD Sewer Infrastructure	IV-16
Exhibit IV.F.2: Existing Metro Water Infrastructure	IV-17
Exhibit IV.H: Arizona State Museum Records Check	IV-19
Exhibit IV.I: Composite Map.....	IV-21

List of Tables

Table II.F.2: Trip Generation	II-9
Table II.F.3: 2023 Daily Volumes (With and Without Project)	II-9
Table IV.E: Roadway Inventory	IV-11

Appendices

- Appendix A: ALTA Survey
- Appendix B: Biological Impact Report
- Appendix C: Traffic Impact Analysis
- Appendix D: PCRWRD Type I Sewer Response
- Appendix E: Conceptual Elevations
- Appendix F: Parking Demand Letter
- Appendix G: Metro Water Letter

Thornydale Storage Specific Plan

I. INTRODUCTION & POLICY

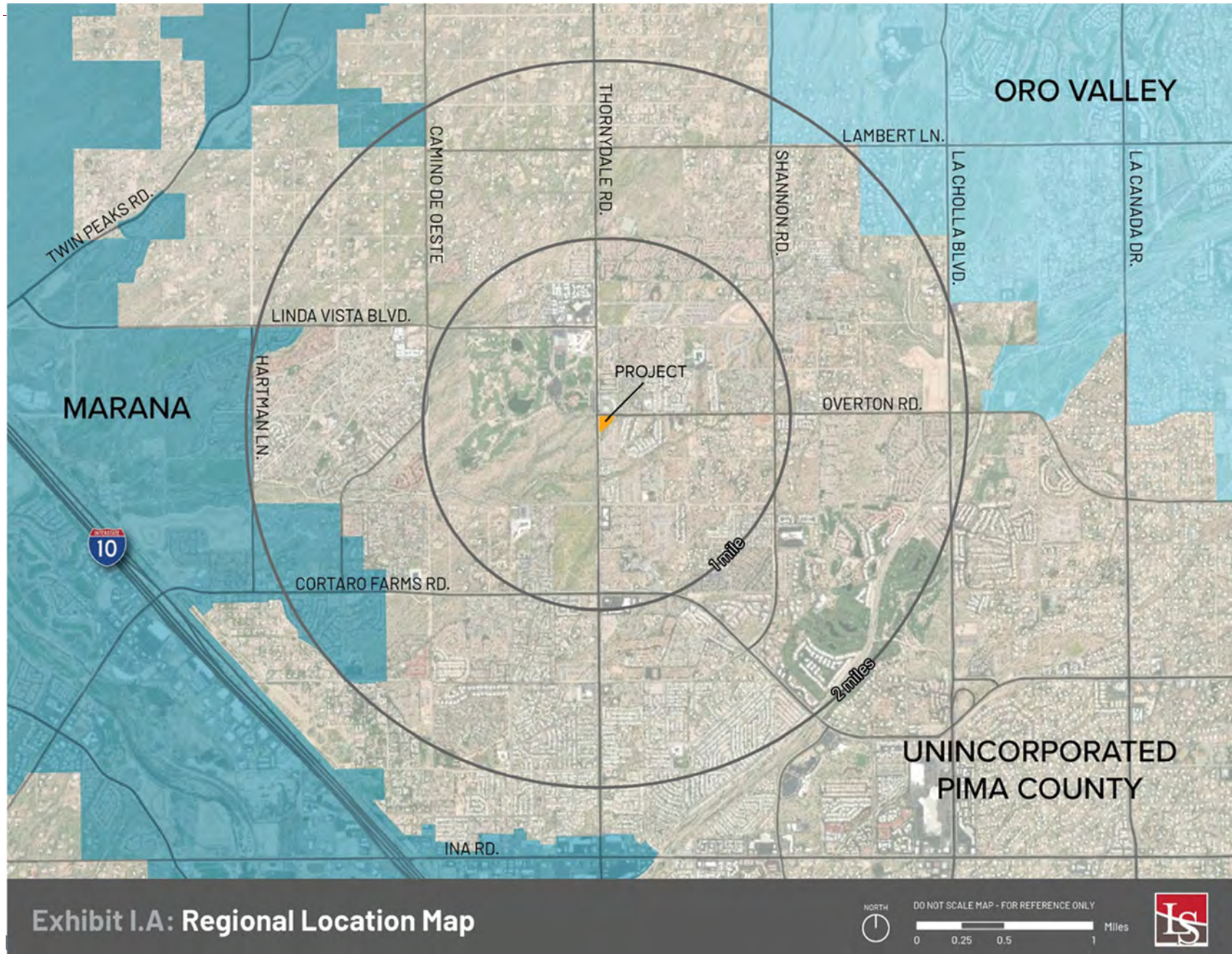
A. Introduction

ZDC Properties, LLC (“ZDC” or “Owner”) is proposing to develop approximately 2.35 acres of land at the southeast corner of Thornydale Road and Overton Road in unincorporated Pima County (“County”), Assessor’s Parcel Number 225-02-027M (the “Property”). (See *Exhibit I.A: Regional Location Map*.)

The Property is split zoned to delineate its developable and non-developable areas. The Property’s development area is zoned Local Business (“CB-1”) and its Comprehensive Plan, *Pima Prospers* (the “Plan”), land use designation is Neighborhood Activity Center (“NAC”). The Property’s protected open space area (established with a prior rezoning) is zoned Multiple Residence (“CR-5”), and its planning land use designation is Medium/Low Intensity Urban (“MLIU”). ZDC proposes to develop the Property’s developable area with a multi-story Self Storage facility (the “Project”). While the County Zoning Code (“PCZC”) permits the self-storage use in CB-1, it restricts the height to a single story and therefore is not sufficient for a modern self-storage building. This height limitation is the sole reason ZDC is requesting a rezoning for the Project.

This document, the Thornydale Storage Specific Plan (“Specific Plan”), serves as the land use proposal for the Project. Section I.B describes how the proposed rezoning conforms with the Plan. Section II, Land Use Proposal, describes the Project proposal, including the permitted uses and applicable development standards. Section III of the Specific Plan describes the Project’s implementation plan and County review process for development of the Project. Section IV, Site Analysis, includes an analysis of the Property’s existing conditions. The final section of the document will include the Specific Plan’s conditions of approval, pursuant to the ordinance adopted by the Board of Supervisors.

Thornydale Storage Specific Plan



Thornydale Storage Specific Plan

B. Compliance with Pima Prospers

The Property is located within the Tortolita Planning Area of the Plan. This Specific Plan is consistent with the intent, goals, and policies of *Pima Prospers*. The following policies from the Plan are relevant and support this Specific Plan:

1. Use of Land

a. Land Use Element

- **Policy 3.1.1.4:** Support land uses, densities, and intensities appropriate for the urban, suburban, and rural areas of the unincorporated County.
- **Policy 3.1.1.6:** Promote a compact form of development in urban and suburban areas where infrastructure is planned or in place and the market is receptive.

b. Environmental Element

- **Policy 3.4.1.3.b:** The following Conservation Guidelines apply to Important Riparian Areas (“IRA”) within the Conservation Lands Systems (“CLS”):

Every effort should be made to protect, restore and enhance the structure and functions of the IRA, including their hydrological, geomorphological and biological functions;
- **Policy 3.4.1.3.c:** The following Conservation Guidelines apply to Important Riparian Areas (“IRA”) within the Conservation Lands Systems (“CLS”):

Areas within an IRA that have been previously degraded or otherwise compromised may be restored and/or enhanced.
- **Policy 3.4.1.6.b:** The following Conservation Guidelines apply to Multiple Use Management Areas (“MUMA”) within the Conservation Lands Systems (“CLS”):

Land use and management goals within these areas focus on balancing land uses with conservation, restoration, and enhancement of native biological communities and must:
 - Facilitate the movement of native fauna and pollination of native flora across and through the landscape;
 - Maximize retention of on-site conservation values; and,
 - Promote landscape integrity.

The CLS designation on the Property is “Multiple Use Management Areas” and “Important Riparian Areas”. The Property has achieved compliance with the Maeveen Marie Behan Conservation Land System Conservation Guidelines (“CLS”) pursuant to the August 7, 2018 rezoning of the Property and surrounding area (Ordinance No. 2018-024, Case No. P18RZ00001, the “2018 Rezoning”). CLS compliance is shown on Thornydale

Thornydale Storage Specific Plan

Preserve Tentative Plat, P18TP00010. This rezoning will maintain all of the protected natural open space designated by the 2018 Rezoning.

- **Policy 3.5.10.2:** Ensure that all new development and redevelopment reflects the character and sense of place of the area.
 2. Encourage development in suburban areas to be integrated with its Sonoran Desert setting by:
 - a. Encouraging a Sonoran Desert color palette that is not limited to earth tones;
 - b. Incorporating the site's prominent existing features (rock formations, etc.) as part of the design, where appropriate;
 - c. Supporting contemporary and energy efficient versions of vernacular architectural styles;
 - d. With the exception of local food production, continue to utilize a drought-tolerant plant palette that emphasizes both the use of the native species and precludes the use of non-native invasive plant species near public preserves and natural open spaces; and,
 - e. Maximize the use of shade devices where most appropriate including planting trees for pedestrians.
- **Policy 3.5.14:** Encourage cost-effective green building and site design methods, techniques and materials.
 1. Decrease heat island effect and reduce water run-off through site development strategies.
 2. Reduce outdoor water use by encouraging water-efficient practices such as:
 - a. Low water use, drought-tolerant or native vegetation (xeriscapes) with the exception of local food production;
 - b. Drip irrigation;
 - c. Increase use of reclaimed water and rainwater harvesting; and
 - d. Low Impact Development (LID) principles such as preserving and recreating natural landscape features and minimizing effective imperviousness to create functional and appealing site drainage that treat stormwater as a resource rather than a waste product where applicable and feasible.
 3. Reduce indoor water use by installing water-efficient fixtures and appliances.
 4. Increase building energy efficiency by encouraging active and passive solar methods of construction.

The Project will feature a variety of energy-efficient, green building and site design elements that are supported by Pima Prosper. These approaches include:

- High-efficiency LED lighting throughout.

Thornysdale Storage Specific Plan

- High Seer air conditioning systems.
- Low-water-use plumbing fixtures.
- Rainwater will be directed to landscape areas for plantings and recharge back into the ground.

Thornydale Storage Specific Plan

II. LAND USE PROPOSAL

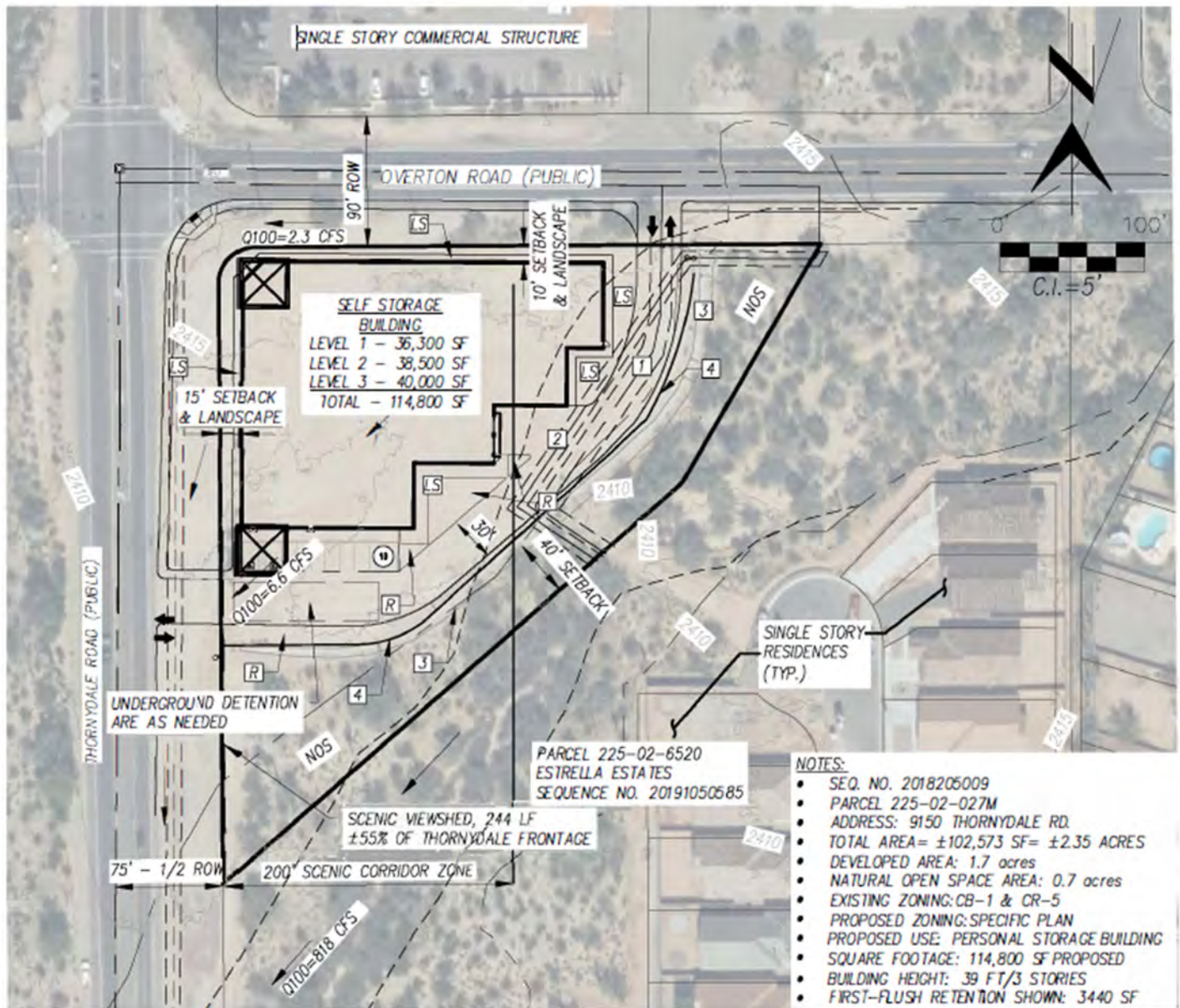
A. Project Overview

The vision for the Property is to develop a state of the art, secure, multi-story self storage facility to address increased market demand in the area. A multi-story self storage facility is an ideal design for urban areas where land is limited and rentable space is maximized to meet increasing demands. The Specific Plan provides the regulatory framework and guidelines for implementing the vision for the proposed development.

All site improvements are illustrated on *Exhibit II.A: Preliminary Development Plan*.

Thornydale Storage Specific Plan

Exhibit II.A: Preliminary Development Plan



LEGEND

- 1 EX. 15' WIDE WATER EASEMENT
- 2 EX. 10' WIDE ELECTRIC EASEMENT
- 3 EXISTING BANK PROTECTION
- 4 5' SITE WALL
- LS PROPOSED LANDSCAPE

LEGEND

- PROPERTY BOUNDARY
- EROSION HAZARD SETBACK
- FLOW DIRECTION
- RIPARIAN
- EXISTING FLOOD PLAIN
- LANDSCAPE AREA WITH RETENTION/DETENTION [R]



Engineering and Environmental Consultants, Inc.

555 E. River Road, Suite 301 | Tucson, Arizona 85704

Tel 520.321.4625 | Fax 520.321.0333

THORNYDALE STORAGE
PRELIMINARY DEVELOPMENT PLAN
9150 N THORNYDALE ROAD
PIMA COUNTY, ARIZONA

JULY 2022

EEC JOB NO. 22013

SHT 1 OF 1



Thornydale Storage Specific Plan

B. Permitted Uses

- All uses within County CB-1 Zone are expressly permitted.
- Additional permitted uses:
 - Multi-story self-storage facility - subject to the following standards:
 - All access to the facility shall be from an arterial or collector street;
 - All storage be in an enclosed building;
 - The structure be a single building, maximum of three (3) stories not to exceed forty (40) feet in height to top of roof and forty-seven (47) feet to top of parapet/architectural features;
 - All access lanes be of a paved or concrete surface;
 - There shall be no razor or barbed wire;
 - No activity occurs other than the loading and unloading of household and personal property.
- In addition, the Pima County Planning Official may permit any use not specifically listed in County CB-1 Zone or this Specific Plan if determined to be similar in nature to other permitted uses.

C. Development Standards

1. Site Development

For the application of development standards, the Property shall be considered a single parcel regardless of the ultimate parcel configuration within the Specific Plan boundary.

The following development standards shall govern site development throughout the Specific Plan. Setbacks shall not for any internal lot lines.

- Minimum Site Area: None
- Minimum Site Width: None
- Maximum Site Coverage: N/A
- Maximum Building Height:
 - Self-Storage Facility: 39 feet
 - Parapet may extend 4 feet above maximum building height.
 - Architectural features may extend up to 8 feet above maximum building height. Architectural features shall be limited to 10 percent or less of total roof area.
 - All other Permitted Uses: Thirty (30) feet
- Minimum Building Separation: per building code
- Minimum Site Setbacks:
 - Front (Street): Fifteen (15) feet on Thornydale Rd.
 - Side (Street): Ten (10) feet on Overton Rd.
 - Rear: Forty (40) feet

2. Landscape Standards

- All existing landscaping shall remain until it is affected by redevelopment.

Thornysdale Storage Specific Plan

- Existing landscaping in the Natural Open Space areas shall remain undisturbed.
- Only the landscaping within the affected Development Area is required to be redesigned to Specific Plan standards as defined below, and a landscape plan shall be submitted for that Development Area with the development package.
- All new Development Areas will comply with the PCZC Chapter 18.73, Landscape, Buffering and Screening Standards with the following exceptions:
 - So long as the proposed habitat disturbance is consistent with the prior CLS compliant rezoning, as show on the approved plans/cases “P18RZ00001”, “P18SC00082”, and “P18TP00010,” the following are in compliance and shall not be subject to further review:
 - Conservation Land System (“CLS”) mitigation compliance;
 - PCZC Chapter 18.72 (“Native Plant Preservation”); and,
 - PCZC Chapter 16.30 (“Watercours and Riparian Habitat Protection and Mitigation Requirements”).
 - PCZC Chapter 18.73.050 (“Amenity Landscaping Requirements”) shall not apply.

3. Vehicular & Bicycle Parking

- All new development will comply with Code Chapter 18.75, Off-Street Parking and Loading Standards with the following exceptions:
 - Parking Spaces Required – Self-storage facility
 - 1 space for any associated office
 - 1 space per ten thousand (10,000) square feet of gross floor area for the first fifty thousand (50,000) square feet of gross floor area
 - 1 space per twenty thousand (20,000) square feet of gross floor area over fifty thousand (50,000) square feet of gross floor area

See Appendix F (Parking Demand Letter) for justification of parking requirements.

4. Loading

- All new development will comply with Code Chapter 18.75, Off-Street Parking and Loading Standards with the following exceptions:
 - Minimum Dimensions – Self-storage facility:
 - Length: Min. Forty (40) feet, Max. None;
 - Width: Min. Twelve (12) feet;
 - Overhead clearance: Min. Fourteen (14) feet.
 - Off-street Loading Spaces Required – Self-storage facility:
 - Combined square feet of floor area and outdoor storage and use areas less than (<) one hundred twenty thousand (120,000) square feet: 2 spaces
 - Each additional fifty thousand (50,000) square feet: 1 space

5. Storage Facility Hours of Operation

- Office: 9:00 a.m. to 6:00 p.m.
- Tenant Access: 6:00 a.m. to 10:00 p.m.

Thornydale Storage Specific Plan

6. Design Standards

A multi-story self-storage facility is intended to have enhanced architectural features that correspond with similar commercial and/or office developments along a major arterial street. The design features of a multi-story self-storage facility shall generally correspond with the conceptual design shown at *Appendix E: Conceptual Design*.

7. Buffelgrass Elimination

While no buffelgrass was observed on the Property, the Owner shall have a continuing responsibility to remove buffelgrass (*Pennisetum ciliare*) from the Property. Acceptable methods of removal include chemical treatment, physical removal, or other known effective means of removal. This obligation also transfers to any future owners of the Property within the Specific Plan area.

D. Scenic Route

- Thornydale Road is considered a Major Scenic Route.
- All new development will comply with Code Chapter 18.77.040.E, Scenic Route Development Standards, with the following exceptions:
 - Building Height – Self-storage facility:
 - Forty (40) feet to top of roof (not to exceed 47 feet with parapet and architectural features)
 - A minimum view corridor width of eighty-eight (88) feet shall be provided.
 - Building and Wall Surfaces – Self-storage facility:
 - An installation of Public Art shall be permitted on a building and wall surface visible from N. Thornydale Rd. or W. Overton Rd.
 - All other building and wall surfaces, shall have colors which are compatible with surrounding natural landscape (desert/earth tones). Allowable colors include: brown – including rusts, sepias, sands, tans and buffs, olive and grey.

E. Hydrology

The Property is approximately 2.35 acres in size with approximately 1.55 acres planned for development.

It is located within a Critical Basin as indicated on the County's 2009 Critical Basin Map. This designation requires the developed condition to include a reduction of 10% of the pre-developed discharges from the site, in addition to First Flush requirements. Under natural (predeveloped) conditions 10.2 cubic feet per second ("cfs") are generated, as computed with using PC-Hydro, the County approved methodology.

Under developed conditions, at an overall imperviousness of 60%, the property would generate approximately 18 cfs. This developed discharge will have to be reduced to 8.67cfs, or 85% of the predeveloped discharge, and require the use of retention/detention basin(s).

Thornydale Storage Specific Plan

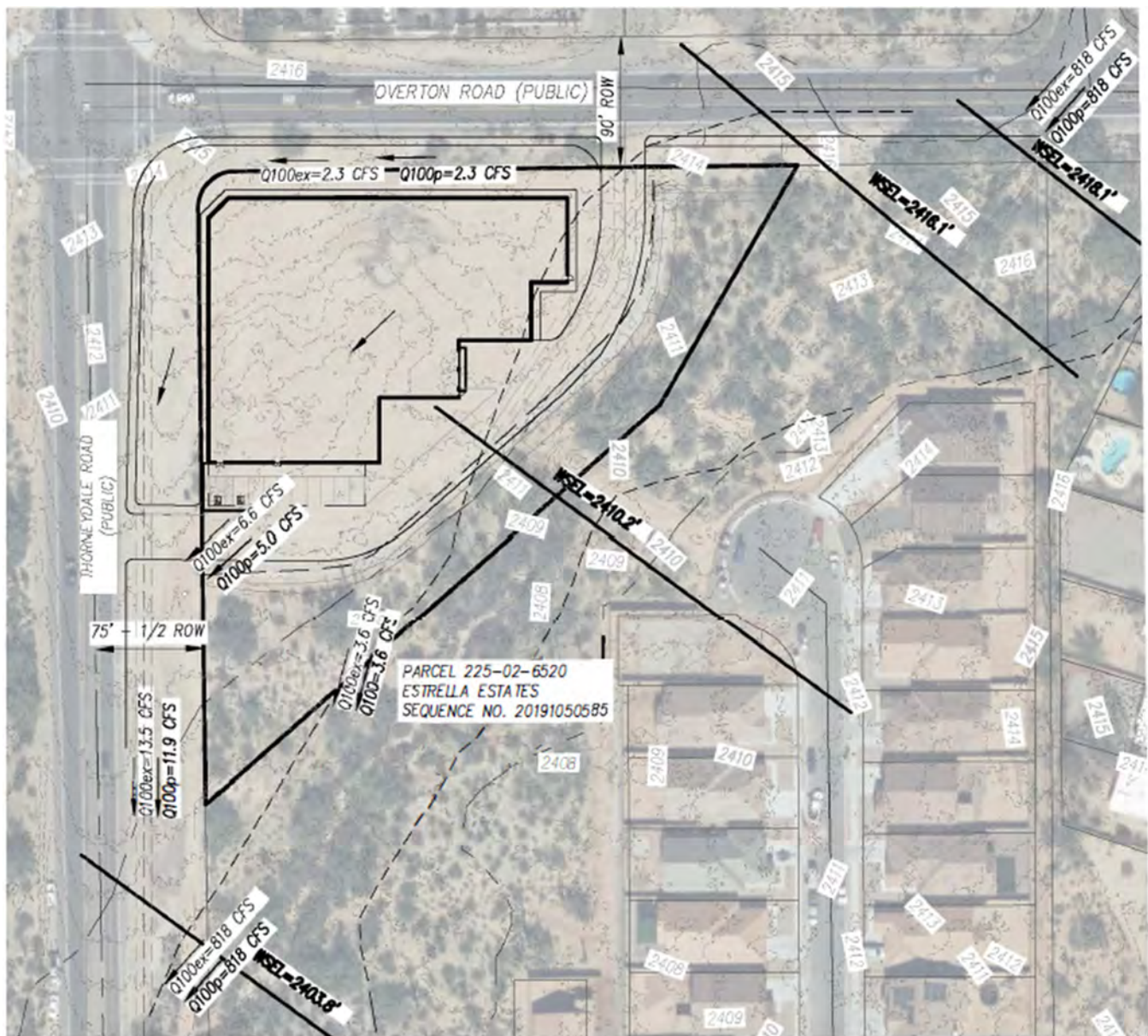
Preliminary basin sizing indicates a minimum volume of 8,820 cubic feet, which includes the First Flush requirement of 2,562 cubic feet.

The construction of the concrete bank protection along the unnamed wash, eliminates the erosion hazard setback for the proposed development site, which is 50 feet per County Standards. LOMR Case # 19-09-1762P establishes the floodplain and water surface elevations of the unnamed wash (818 cfs) for the Property - which range from an elevation of approximately 2416.1 feet at the north and 2403.8 feet to the south.

See *Exhibit II.E: Drainage Concept*.

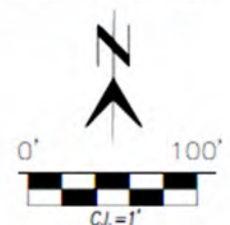
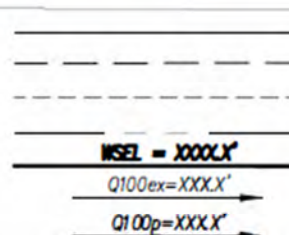
Thornydale Storage Specific Plan

Exhibit II.E: Drainage Concept



LEGEND

- PROPERTY BOUNDARY
- EROSION HAZARD SETBACK
- RIPIARIAN
- EXISTING FLOOD PLAIN
- EXISTING FLOOD SECTIONS
- EXISTING DISCHARGES
- PROPOSED DISCHARGES



Engineering and Environmental Consultants, Inc.

555 E. River Road, Suite 301 | Tucson, Arizona 85704

Tel 520.321.4625 | Fax 520.321.0333

THORNYDALE STORAGE
EXHIBIT II.E: DRAINAGE CONCEPT
9150 N THORNYDALE ROAD
PIMA COUNTY, ARIZONA

MAY 2022

EEC JOB NO. 22013

SHT 1 OF 1



Thornydale Storage Specific Plan

F. Transportation and Circulation

1. Proposed Ingress/Egress and Onsite Vehicular Circulation

Access is proposed at two location for the Property; one on Thornydale Road approximately 270 feet south of the Overton Road nearside pavement edge; and one on Overton Road approximately 340 feet east of the Thornydale Road pavement edge, and directly opposite from an existing driveway for the Thornydale Village shopping center.

Based on the low trip generation, dedicated turn lanes are not expected to be warranted on Thornydale Road or on Overton Road at the proposed Project access locations. The driveway on Thornydale Road may provide a potential conflict with the northbound left turn lane at the Thornydale/Overton intersection, and limited movements maybe be required.

Circulation will be as shown on the preliminary site plan with access from both entrances. A thirty-foot drive will allow two-way traffic on-site. Parking will be located near the Thornydale Road.

See Exhibit II.A: Preliminary Development Plan

2. Average Daily Traffic & Concurrency

The Project is expected to add approximately 166 new external vehicle trips per day, based on average trip rates for the lane use “Mini Warehouse”.

See Table II.F.2: Trip Generation.

Distributing these trips to the study area roadways based on existing traffic patterns and adding them the future background volume for the year 2023 results in the total daily volumes shown in Table II.B.

See Table II.F.3: 2023 Daily Volumes (With and Without Project).

Thornydale Storage Specific Plan

Table II.F.2: Trip Generation

Trip Generation Rates			AM Peak		PM Peak		Weekday	
Land Use	1000 SF	ITE Code	In	Out	In	Out	In	Out
Mini-Warehouse	114.8	151	0.09		0.15		1.45	
			59%	41%	47%	53%	50%	50%
Trip Generation			AM Peak		PM Peak		Weekday	
Land Use	1000 SF	ITE Code	In	Out	In	Out	In	Out
Mini-Warehouse	114.8	151	10		17		166	
			6.36	4.42	8	10	87	87

Table II.F.3: 2023 Daily Volumes (With and Without Project)

Street	Daily Capacity at LOS D*	2023 ADT No Project	Site Trips	2023 ADT With Project	Over LOS D Capacity (No Project)	Over LOS D Capacity (With Project)
Camino de Oeste						
Lambert Lane to Linda Vista	10,660	1,283	0	1,283	No	No
Linda Vista to Bald Eagle	10,660	5,147	0	5,147	No	No
Thornydale Road						
Pecos Drive to Linda Vista	16,730	15,517	70	15,587	No	No
Linda Vista to Overton	16,730	20,302	70	20,372	Yes	Yes
Overton to Hardy	16,730	17,641	70	17,711	Yes	Yes
Hardy to Cortaro Farms	16,730	19,004	70	19,074	Yes	Yes
Shannon Road						
Linda Vista to Overton	12,740	8,010	17	8,027	No	No
Overton to Hardy	12,740	8,074	17	8,091	No	No
Linda Vista Boulevard						
Camino de Oeste to Thornydale Road	13,990	11,419	0	11,419	No	No
Thornydale Road to Shannon Road	13,990	3,054	0	3,054	No	No
Overton Road						
West of Thornydale	10,660	1,411	0	1,411	No	No
Thornydale Road to Shannon Road	10,660	8,126	35	8,160	No	No
Hardy Road						
West of Thornydale	10,660	1,924	0	1,924	No	No
East of Thornydale	10,660	1,787	0	1,787	No	No
*Generalized Annual Average Daily Volumes for Florida's Urbanized Areas, from 2020 FDOT Quality/Level of Service Handbook Tables						

Thornydale Storage Specific Plan

Traffic volumes on the Project roadways are expected to be below the daily LOS D threshold volumes for all roadways except Thornydale Road. The addition of site trips is projected to be very minor compared to the background traffic volumes.

The projected ADTs in 2023 on Thornydale Road currently exceed their daily LOS D volume threshold for a two-lane roadway. Although not on any current Pima County improvement programs, the widening of Thornydale Road had previously been on older roadway construction programs, such as the PAG Transportation Improvement Program. All other roadways currently, and will operate, at LOS D or better with the Project trips added.

3. Distances to Existing Drives/Intersections

Distances from the Property to existing driveways and intersections are shown in *Exhibit II.F: Distances to Existing Driveways/Intersections* and are listed below.

4. Bicycle & Pedestrian Circulation

No bicycle or pedestrian infrastructure is planned for this Project. A Self-storage facility land use designation does not typically draw customers who are pedestrians or are on bikes.

5. Traffic Impact Analysis

See Appendix C: Traffic Impact Analysis

G. Utility Infrastructure

1. Sewer

The site will connect to the existing sewer system G-84-024 at existing manhole 4201-16. This manhole has adequate depth to sever the property and is located in an area that will avoid the need for removal/replacement of existing pavement. If the depth of this sewer, approximately twenty (~20') feet proves to be problematic, an alternative connection location on Overton Road near MH 4201-17-1 is available. This connection point requires a pavement patch across Overton Road, and is a much shallower manhole at approximately ten (~10') feet. *See Appendix D: Pima County Regional Wastewater Reclamation Department Type I Capacity Response.*

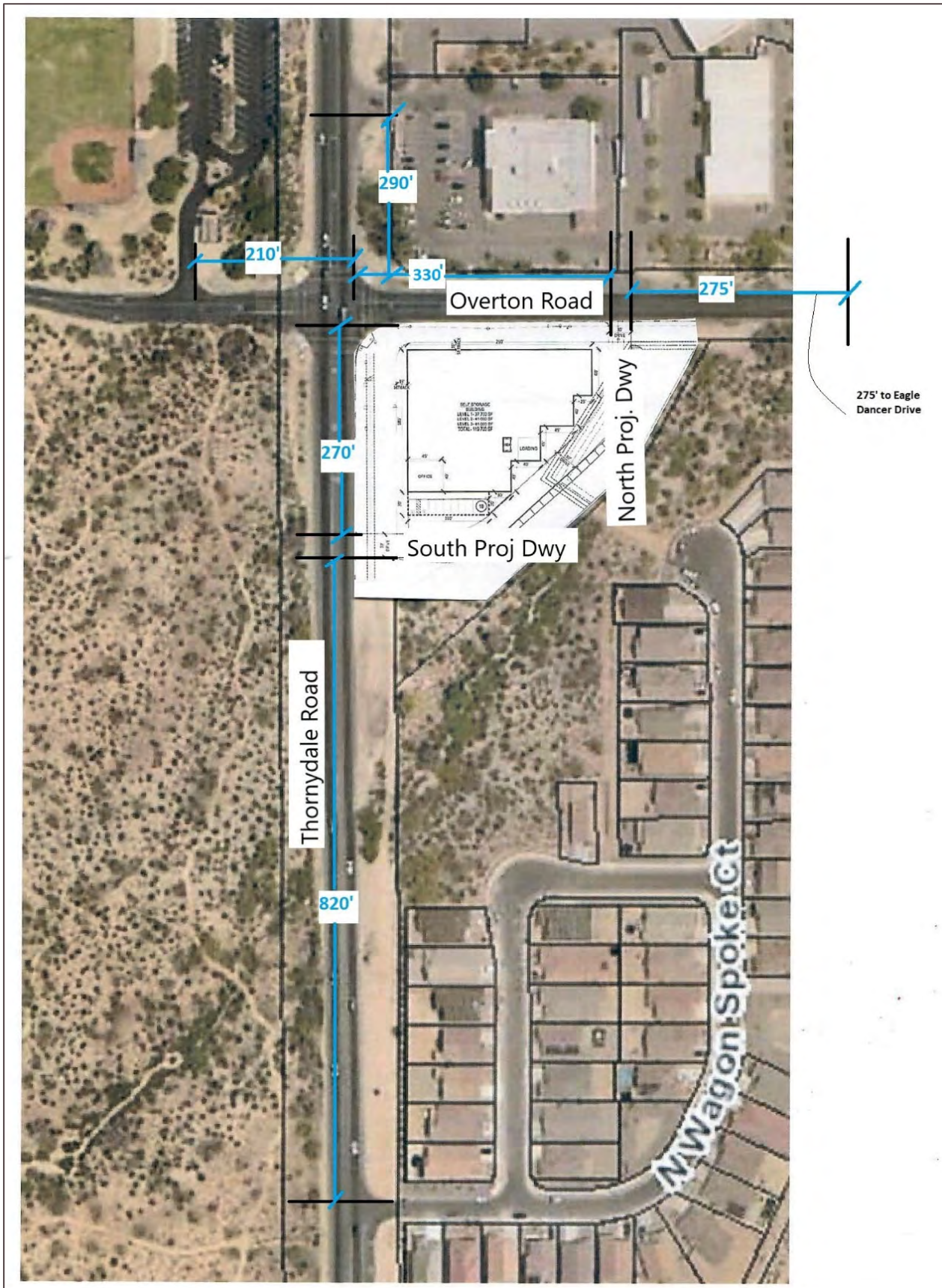
2. Water

Water service will be provided by Metro Water which has a 100-year assured water supply. The property lies within the Metro Water service area. *See Appendix G: Metro Water Letter.*)

Metro Water has an 8" PVC mainline that cuts through the northeast portion of the parcel in a 15' wide public water easement. This 8" PVC mainline will serve as the connection point for this new development.

Thornycroft Storage Specific Plan

Exhibit II.F: Distances to Existing Driveways/Intersections



III. IMPLEMENTATION AND ADMINISTRATION

A. Administration and Interpretation

The Specific Plan shall be administered by the Pima County Planning Official, and all implementation decisions shall be based on the purpose of the Specific Plan. If a conflict arises between the Specific Plan and the PCZC, the Specific Plan shall control. If the Specific Plan is silent on any issue, and the PCZC is consulted, the purpose and intent of the Specific Plan shall control the Planning Official's decision whether and how to apply the PCZC. Appeals of any Planning Official interpretation of this Specific Plan may be made to the Board of Adjustment within 30 days of the date of the interpretation. A fee in accordance with adopted Pima County Development Services Department Fee schedule for an "Appeal of an Interpretation" and an "Advertised Public Hearing" must accompany any such appeal. The Specific Plan will not result in the modification or change of any existing County-adopted building codes.

B. Phasing and Procedures for Development Review

This Project is intended to be built in a single phase. A development package will be submitted after the Specific Plan is approved and will reflect all onsite and offsite improvements in conformance with the Specific Plan and conditions of approval. (See Section V.) Detailed traffic and hydrology reports will be submitted with the development package, if required.

The Owner serves as the Master Association and Property Manager for the Specific Plan. Through a self-certification process, the Owner shall review and approve all development packages/Project design features, signage applications, and architectural/building plans proposed for the Property prior to County submittal.

C. Amendments

1. Minor

The County Planning Official may administratively approve minor (or insubstantial) changes, as defined below, to the Specific Plan, provided such changes are in conformance with the overall intent, goals and objectives of the Specific Plan as presented herein.

The following shall be considered minor changes that fall within the administrative purview of the Planning Official:

- Addition of new information to the Specific Plan, maps or text otherwise in compliance with the below standards.
- Changes to the public or private infrastructure as presented herein as necessary to properly serve the Specific Plan.

Thornycroft Storage Specific Plan

- Addition of permitted uses that may not be specifically listed in Section II.C of this Specific Plan, but which are determined to be sufficiently similar in type and nature to those listed as permitted.
- Adjustments to the Development Standards in Section II.D of this document that are not harmful to the interests of the larger community or adjacent properties, or which are not explicitly stated in the Specific Plan, but which are consistent with the guiding goals and objectives of the Project and do not create any public health or safety issues.
- Adjustments to any aspect of Section II of this Specific Plan that is required to comply with changes in local, state or federal safety and/or health codes.

2. Major

Major (or substantial) amendments to the Specific Plan shall be those changes or modifications that materially alter the guiding goals and objectives as presented in the Specific Plan. Major amendments to the Specific Plan shall be processed in accordance with the PCZC, Section 18.90.080.

Thornydale Storage Specific Plan

IV. SITE INVENTORY

A. Land Use

1. Location/Regional Context

The 2.35-acre Property is located at the southeast corner of N. Thornydale Road and W. Overton Road within Township 12 South, Range 13 East, Section 20 in unincorporated Pima County. (See *Exhibit I.A: Regional Location Map* provided in previous section.)

2. Existing Land Uses

Existing land uses are identified on *Exhibit IV.A.2: Existing Zoning & Land Use*.

a. Onsite Land Uses - Vacant

b. Offsite Land Uses

- 1) North – Commercial
- 2) South – Single-Family Residential
- 3) East – Single-Family Residential
- 4) West – Open Space/Recreation

3. Existing Zoning

As indicated on *Exhibit IV.A.2: Existing Zoning & Land Use*.

a. Property Zoning – Local Business (“CB-1”) and Multiple Residence (“CR-5”)

b. Surrounding Zoning:

- 1) North – Local Business (“CB-1”)
- 2) South – Multiple Residence (“CR-5”)
- 3) East – Multiple Residence (“CR-5”)
- 4) West – Suburban Ranch (“SR”)

Thornysdale Storage Specific Plan



Thornydale Storage Specific Plan

4. Existing Easements

There are two easements associated with the property. The first is a 10' Tucson Electric Power ("TEP") power line easement centered on an existing power line that enters the Property at the northeast corner and bisects the Property before exiting at the rear of the Property to connect to cul-de-sac at the end of N Wagon Spoke Court.

The second is a 15' Public Water Easement that enters the Property at the northeast corner and bisects the Property before exiting at the rear of the Property to connect to the cul-de-sac at the end of N Wagon Spoke Court.

The exact location of these easements are known and are illustrated on *Exhibit II.A: Preliminary Development Plan*.

The proposed development will not have any adverse impact on these easements.

5. Comprehensive Plan

This Property and all surrounding properties are located within the Tortolita, Planning Area 11, of *Pima Prospers*. The Comprehensive Plan Land Use Intensities of this and surrounding properties are listed below and shown in *Exhibit IV.A.5: Comprehensive Plan Land Use Map*.

Property:	Neighborhood Activity Center (NAC) & Medium Low Intensity Urban (MLIU)
North:	Neighborhood Activity Center (NAC)
South:	Medium Low Intensity Urban (MLIU)
East:	Medium Low Intensity Urban (MLIU)
West:	Resource Conservation (RC)

Thornysdale Storage Specific Plan



Thornysdale Storage Specific Plan

B. Topography & Grading

1. Topographic Characteristics

The Property's topography is relatively flat and slopes down in a southwesterly direction towards the southwest corner of the site with an average cross slope of approximately two and a half (~2.5%) percent. The site contains no restricted peaks or ridges, nor does it contain any rock outcrops or talus slopes. There are no slopes over fifteen (15%) percent on the site, or other significant topographic features of note.

Approximately 1.7 acres of the site has been previously disturbed as part of a previous development. This previously-disturbed area consists of the northwesterly portion of the site, and forms a "pad" on the site with existing bank protection along the southeast portion of the site. The area that has not been previously disturbed forms a "strip" of land along the southeasterly portion of the site.

The Property's existing topography is depicted on *Exhibit IV.C: Existing Conditions Hydrology*.

C. Hydrology

1. Offsite Watershed

The off-site contributory watershed is approximately 318 acres and discharges a FEMA reported 818 cfs at the north boundary of the site as an unnamed wash. This watershed is long and narrow with headwaters just south of Ironwood Ridge High School at an approximate elevation of 2665 feet. This watershed slopes southwest to the Property for approximately 2.89 miles. The northern portion is primarily large lot residential with the southern portion being primarily medium density subdivisions. Only minor offsite discharges are expected from the right-of-way of Overton Road and Thornysdale Road.

2. Onsite Hydrology

This Project is located within a Critical Basin as indicated on the County's 2009 Critical Basin Map. This property is approximately 2.35 acres with 1.55 acres planned for development. Flow crosses the site in a south westerly direction and discharges to an existing dip section over Thornysdale Road, approximately 730 feet south of the intersection of Thornysdale Road and Overton Road.

Bank protection along the west bank of the unnamed wash reduced the associated erosion hazard setback and the FEMA SFHA boundaries through LOMR Case # 19-09-1762P, accepted April 20, 2020. The LOMR establishes the floodplain and water surface elevations of the unnamed wash for the property, which range from approximately 2416.1 feet at the north and 2403.8 feet to the south. The Property under natural (predeveloped) conditions generates 10.2 cfs, as computed with PC-Hydro, the County approved methodology.

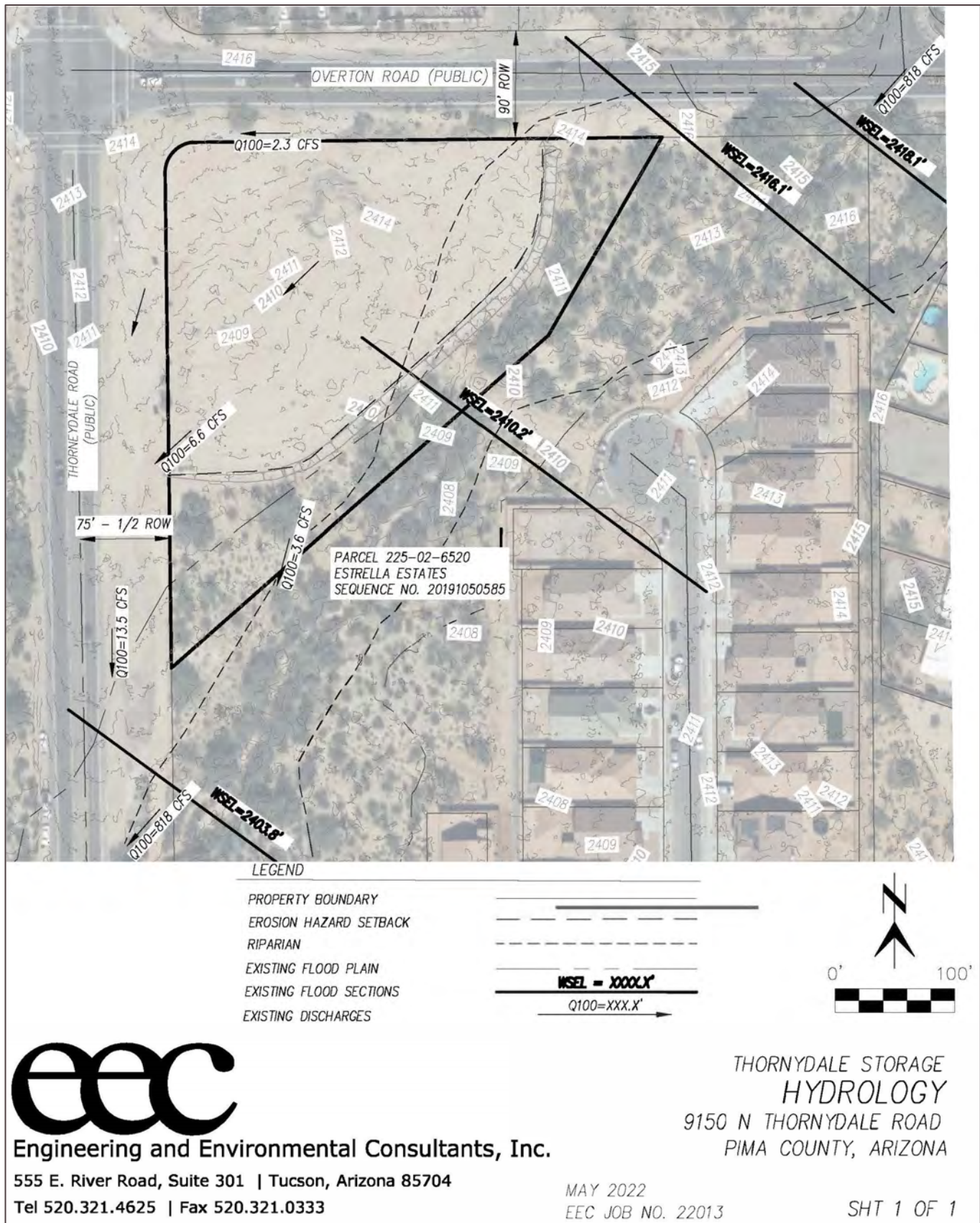
Thornysdale Storage Specific Plan

See *Exhibit IV.C.: Existing Conditions Hydrology*.

3. Floodplain

According to the Federal Emergency Management Agency Flood Insurance Rate Map Panel No. 04019C1660L, dated June 16, 2011, the non-developable area of the Property is located in within a FEMA Flood Zone X and AE with Base Flood Elevations determined. In addition the Property was a part of a Letter of Map Revision (“LOMR”) Case No. 19-09-1762P, dated April 20, 2020.

Exhibit IV.C: Existing Conditions Hydrology



Thornydale Storage Specific Plan

D. Biological Resources

1. Conservation Lands System

The County's Conservation Lands System ("CLS") identifies this Property as "Multiple Use Management Areas" and "Important Riparian Areas". The "Multiple Use Management Areas" are "those areas where biological values are significant, but do not attain the level associated with Biological Core Management Areas. They support populations of vulnerable species, connect large blocks of contiguous habitat and biological reserves, and support high value potential habitat for three or more priority vulnerable species." The "Important Riparian Areas" are "critical elements of the Sonoran Desert where biological diversity is at its highest. These areas are valued for their higher water availability, vegetation density, and biological productivity."

CLS compliance was addressed by plans/cases P18RZ00001, P18SC00082, and P18TP00010. No further compliance is required.

See *Exhibit IV.D: Biological Map*.

2. Priority Conservation Area

a. *Pima Pineapple Cactus*

The Property is not within Priority Conservation Area ("PCA") for the Pima Pineapple Cactus.

b. *Needle-Spined Pineapple Cactus*

The Property is not within PCA for the Needle-Spined Pineapple Cactus.

c. *Cactus Ferruginous Pygmy Owl & Burrowing Owl*

The Property is within the Pygmy Owl Survey Zone 1 PCA. The Property has not been surveyed and no surveys are planned for the future.

Onsite and offsite mitigation and preservation were addressed as part of P18SC00032.

The Property is not within the Burrowing Owl PCA.

d. *Saguaros & Ironwood Trees*

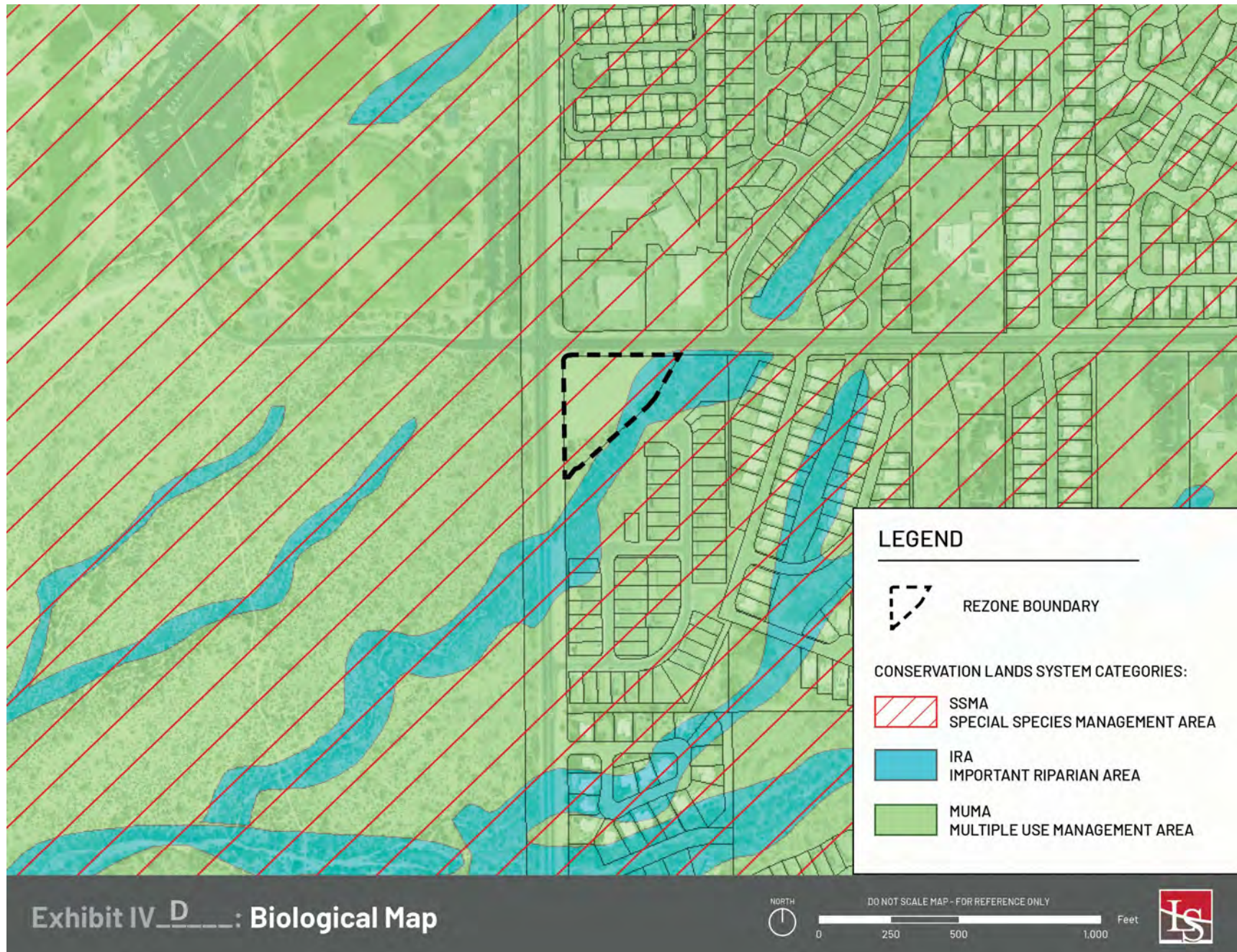
A total of thirty (30) saguaros were identified, seventeen (17) of which were six (6) feet in height or less, and thirteen (13) which were greater than six (6) feet in height. A total of eight (8) ironwood trees were identified on the Property.

Onsite and offsite mitigation and preservation were addressed as part of P18SC00032.

3. Habitat Protection/Community Open Space

Per the Sonoran Desert Conservation Plan, the site is not identified for habitat protection or community open space. There have been no discussions between the Property owner and the County regarding potential acquisition of the Property.

Thornydale Storage Specific Plan



Thornydale Storage Specific Plan

E. Transportation

1. Existing/Planned Offsite Streets

Table IV.E: Roadway Inventory shows the recorded or estimated weekday daily volumes from the Pima Associated Governments website, existing right-of-way widths, the number of lanes and posted speed limits, daily volume capacity at level of service (LOS) D, bike lanes and pedestrian ways for roadways within one mile of the Project.

A short description of the physical characteristics of the major streets near the Project is provided after *Table IV.E: Roadway Inventory*, and the street network is depicted in *Exhibit IV.E: Existing Transportation Network*.

There are no known roadway improvements underway or scheduled in the study area within the next five years.

Thornydale Storage Specific Plan

Table IV.E: Roadway Inventory

Street	Weekday Daily Volume	Data Year	Source	ROW Width (ft)	No. Thru Lanes	Speed Limit	Sidewalks	Bike Route	Daily Capacity at LOS D*	2023 ADT No Project	Site Trips	2023 ADT With Project	Over LOS D Capacity (No Project)	Over LOS D Capacity (With Project)
Camino de Oeste														
Lambert Lane to Linda Vista	1,233	2021	PAG	35-105	2	35	No	No	10,660	1,283	0	1,283	No	No
Linda Vista to Bald Eagle	4,947	2021	PAG	75-90	2	35	Partial	Partial	10,660	5,147	0	5,147	No	No
Thornydale Road														
Pecos Drive to Linda Vista	15,213	2022	FDS	100-145	2	40	Some, East Side	No	16,730	15,517	70	15,587	No	No
Linda Vista to Overton	19,514	2021	PAG	150	2	40	No	No	16,730	20,302	70	20,372	Yes	Yes
Overton to Hardy	16,956	2021	PAG	150	2	40	No	No	16,730	17,641	70	17,711	Yes	Yes
Hardy to Cortaro Farms	18,266	2021	PAG	100-150	2	40	No	Yes	16,730	19,004	70	19,074	Yes	Yes
Shannon Road														
Linda Vista to Overton	7,699	2021	PAG	110	2	40	No	No	12,740	8,010	17	8,027	No	No
Overton to Hardy	7,760	2021	PAG	100-150	2	40	No	No	12,740	8,074	17	8,091	No	No
Linda Vista Boulevard														
Camino de Oeste to Thornydale Road	10,976	2021	PAG	120-150	2	25-35	South Side by MV High Sch	Yes	13,990	11,419	0	11,419	No	No
Thornydale Road to Shannon Road	2,935	2021	PAG	105-135	2	35	Yes	Yes	13,990	3,054	0	3,054	No	No
Overton Road														
West of Thornydale	1,356	2021	PAG	N/A	2	15	No	No	10,660	1,411	0	1,411	No	No
Thornydale Road to Shannon Road	7,810	2021	PAG	75-115	2	40	No	No	10,660	8,126	35	8,160	No	No
Hardy Road														
West of Thornydale	1,849	2021	PAG	45	2	35	No	South Side	10,660	1,924	0	1,924	No	No
East of Thornydale	1,718	2021	PAG	80	2	35	No	No	10,660	1,787	0	1,787	No	No

Thornydale Storage Specific Plan

a. Thornydale Road

Thornydale Road is a nine-mile north/south paved roadway from its northern paved terminus north of Moore Road to its southern terminus at River Road. It is a two-lane arterial road with a two-way left turn lane in the vicinity of the Project and is classified as an urban minor arterial in ADOT's Functional Classification system.. It is classified as a medium volume arterial with a 150-foot right of way in the Pima County Major Streets Plan and as a Scenic, Major Route in the Pima County Scenic Routes Plan.

In the vicinity of the Project, Thornydale Road has a 45-mph speed limit. It has unpaved shoulders along the Project frontage. It is classified as a minor arterial. There are some sidewalks on the east side between Pecos Drive and Linda Vista Boulevard. There are no bike routes along the frontage of the Project site.

b. Overton Road

Overton Road is a two-lane east/west divided with a two-way left turn lane roadway that, on the east side of Thornydale Road, is classified as an urban major collector in ADOT's Functional Classification system. It is classified as a low volume arterial with a 90-foot right of way in the Pima County Major Streets Plan. On the west side of Thornydale Road, it continues as a two-lane undivided park access road into the Arthur Pack Regional Park.

Its speed limit is 40 mph. There are no sidewalks, bike lanes or bus routes along the Project frontage.

c. Shannon Road

Shannon Road is a two-lane north/south undivided paved roadway that is classified as a minor arterial along the Project frontage. It is classified as a low volume arterial with a 90-foot right of way in the Pima County Major Streets Plan and as a Scenic, Major Route in the Pima County Scenic Routes Plan.

Its speed limit is 40 mph. There are no sidewalks, bike lanes or bus routes in the vicinity of the Project.

d. Linda Vista Boulevard

Linda Vista Boulevard is a two-lane east-west paved roadway. It is classified as a medium volume arterial with a 150-foot right of way in the Pima County Major Streets Plan west of Shannon Road and as a low volume arterial with a 90-foot right of way east of Shannon Road. Between Thornydale Road and Shannon Road, it is also classified as a Scenic, Major Route in the Pima County Scenic Routes Plan.

West of Thornydale Road, Linda Vista Boulevard has a two-way left turn lane. East of Thornydale Road, the road continues with a two-way left turn lane then narrows to have one lane in each direction for a short segment and then widens again to have a center turn lane on its approach to Shannon Road. It continues as a local road at Shannon Road to the east. West of Thornydale Road, the posted speed limit is 25 mph and east of Thornydale Road, the posted speed limit is 35 mph.

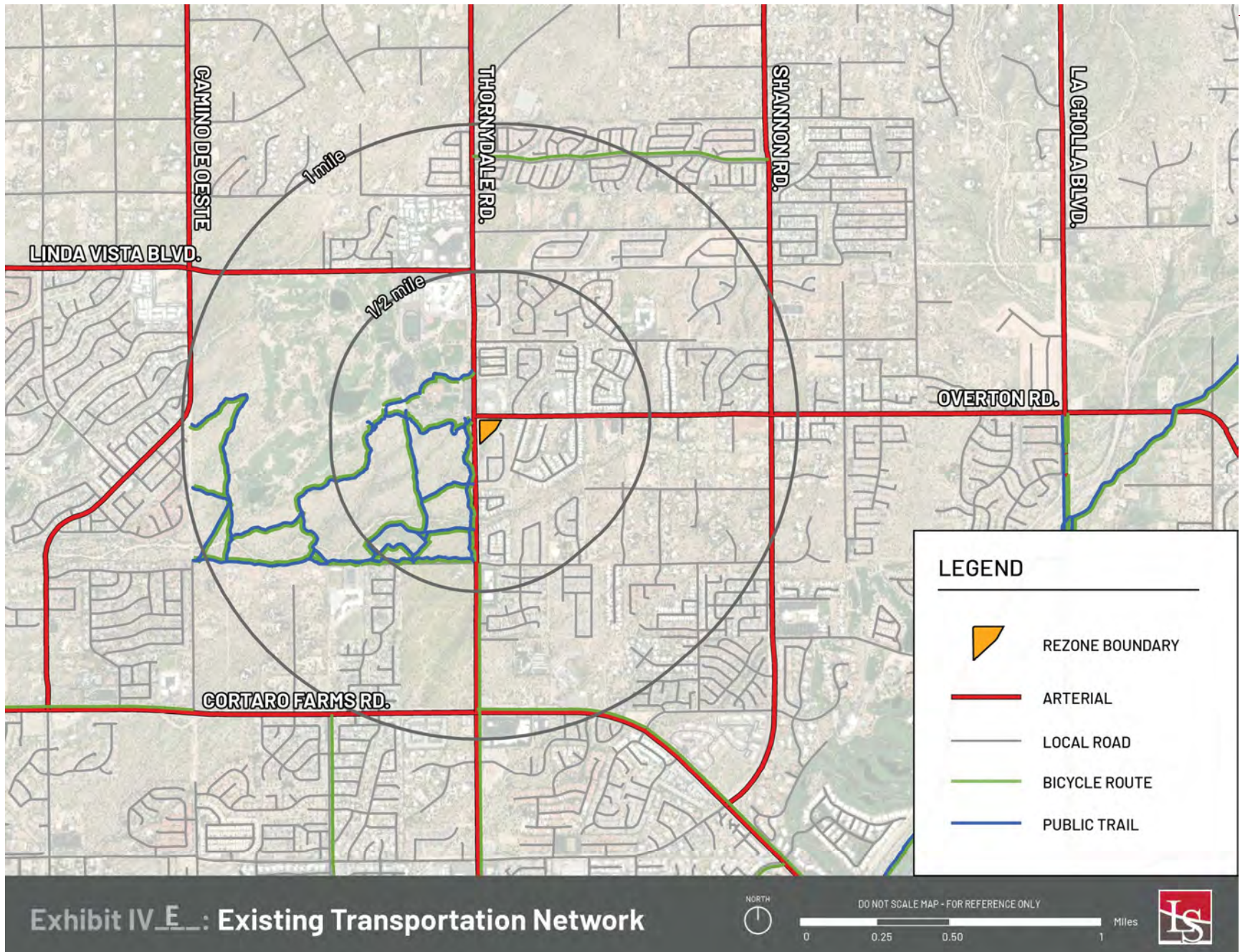
Thornysdale Storage Specific Plan

There are sidewalks and bike lanes along Linda Vista Boulevard within the study area.

2. Public Transit & Alternate Modes

There are no public transit facilities or bus stops serving the Property or surrounding area. The Suntran 412 Sun Shuttle route provides route service north/south along Thornysdale Road, however, the closet bus stop is over a half mile to the north at the intersection of Thornysdale Road and Linda Vista Boulevard. There are no sidewalks or bicycle routes in the Project vicinity.

Thornydale Storage Specific Plan



Thornydale Storage Specific Plan

F. Utilities

1. Sewer

There is an existing 21-inch gravity sewer line per Pima County plan G-84-024 that lies within the eastern half of the Thornydale Road right-of-way, west of the site. A direct connection to an existing 21" sewer main is not typically allowed per PCRWRD standards, so a connection to one of the existing manholes in the area is preferable (near southwest corner of site, MH 4201-16. This sewer is very deep (+/- 20').

There is an existing 8" sewer on the north side of the Overton Right-of-way, north of the site (G-84-024). This sewer is another option for tie-in location, but may not have the depth available to serve the site.

See Exhibit IV.F.1: Existing PCRWRD Sewer Infrastructure.

2. Water

Metro Water has an 8" PVC mainline that cuts through the northeast portion of the parcel in a 15' wide public water easement. This 8" PVC mainline will serve as the connection point for this new development. Metro Water also has an existing transmission main adjacent to the property in Thornydale Road and Overton Road, however this transmission main is not available for distribution.

Tucson water also has existing watermains within both Thornydale Road and Overton Road adjacent to the site, however, the property is located outside of Tucson Waters Obligated Service Area.

See Exhibit IV.F.2: Existing Metro Water Infrastructure.

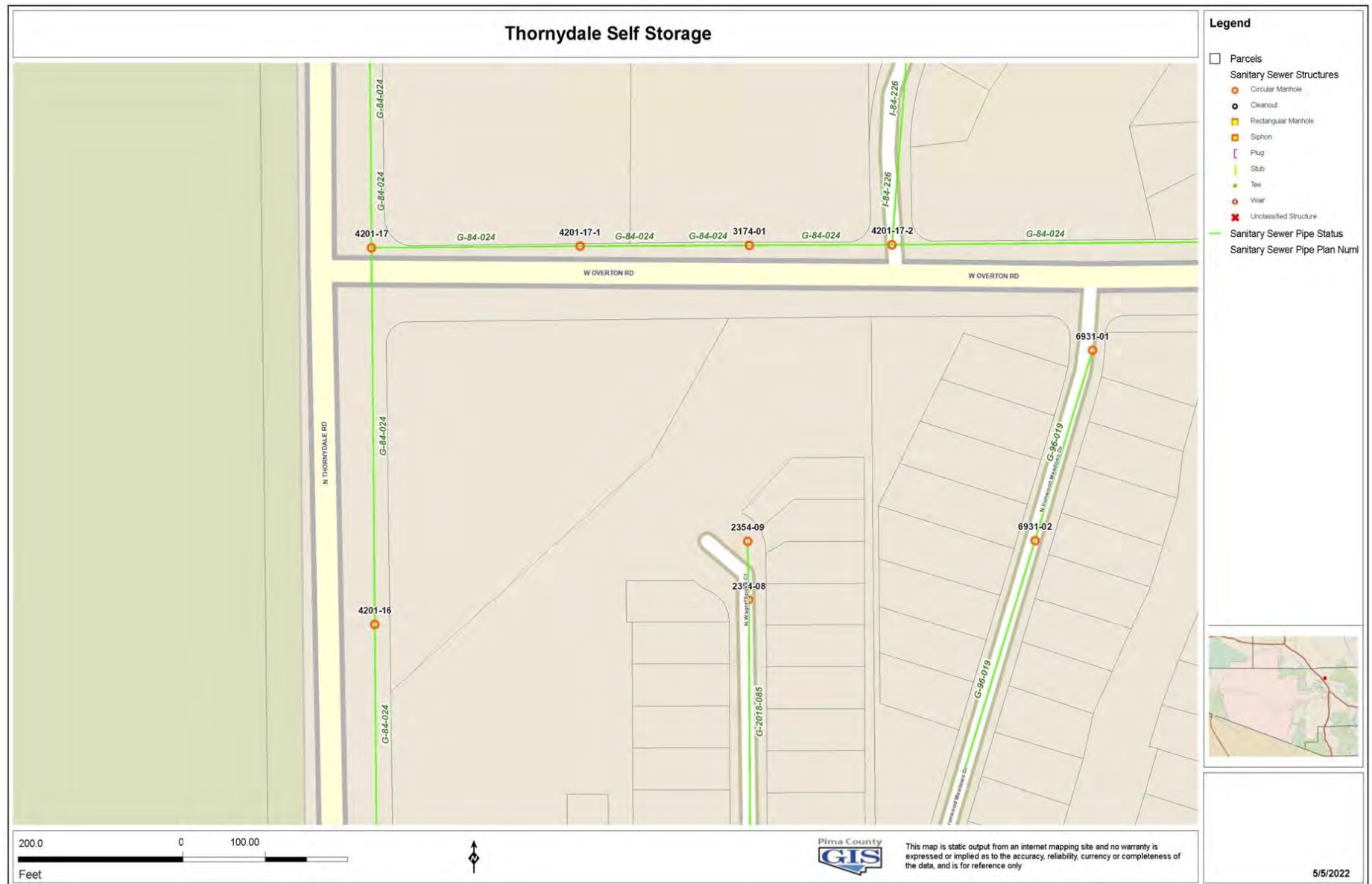
G. Recreation

There are no existing recreational facilities onsite.

Arthur Pack Regional Park, a 500-acre County-owned park, is adjacent to the Property to the west and wraps around the south and west sides of Mountain View High School at the corner of Thornydale Road and Linda Vista Boulevard. The park includes the Crooked Tree Golf Course, playground equipment, picnic areas, multiple sports fields and a 4.7-mile network of pedestrian, bike and equestrian trails through the Maeveen Behan Desert Sanctuary. Vehicular access into the park is provided at Thornydale Road and Overton Road.

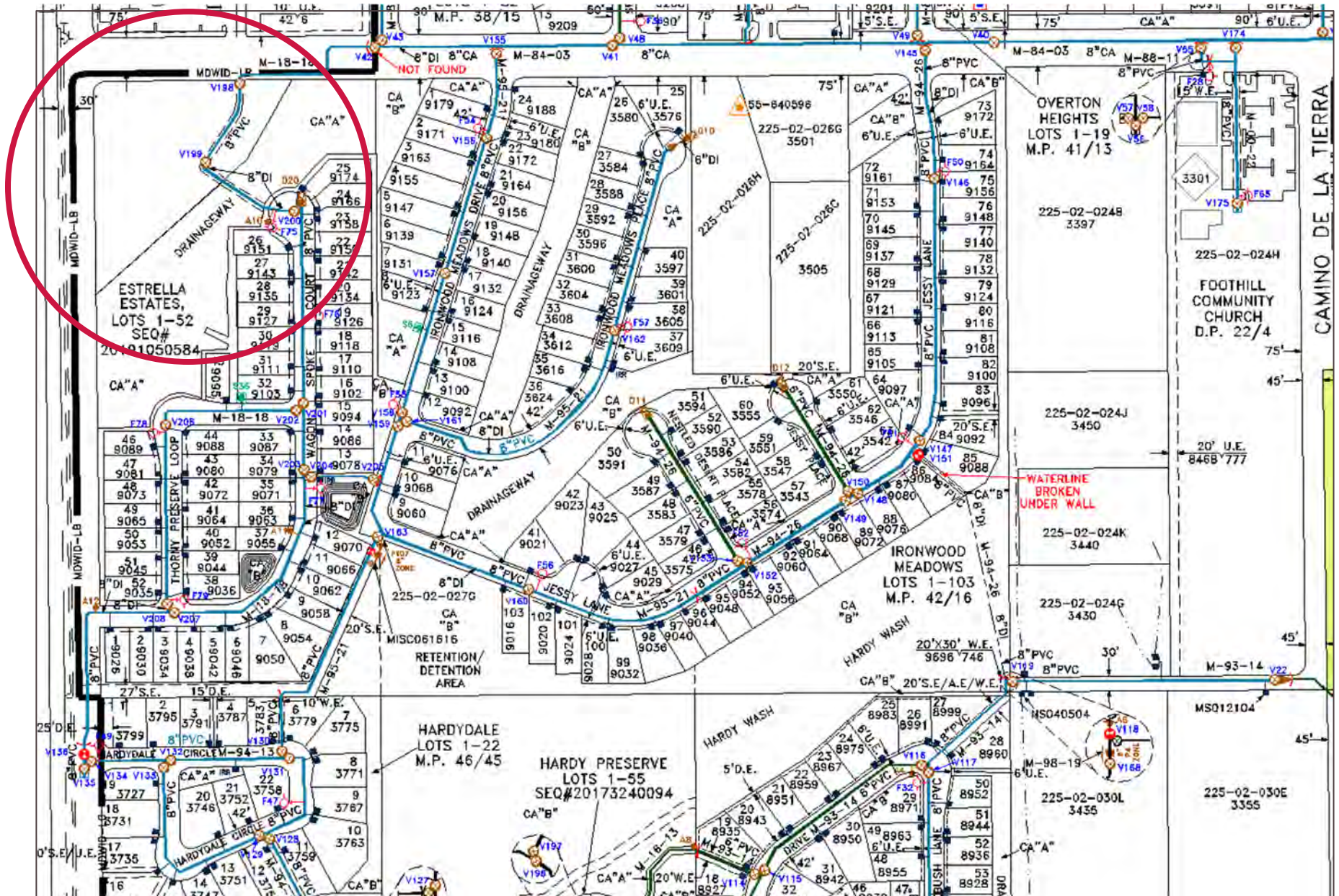
Thornydale Storage Specific Plan

Exhibit IV.F.1: Existing PCRWRD Sewer Infrastructure



Thornycdale Storage Specific Plan

Exhibit IV.F.2: Existing Metro Water Infrastructure



Thornydale Storage Specific Plan

H. Cultural Resources

Correspondence from the Arizona State Museum is included as *Exhibit IV.H*.

Per correspondence with the County's Office of Sustainability, Cultral Resources & Historic Preservation Division, previous archaeological surveys covered this area, so no new survey is required. Development of the Property is subject to ARS § 41-865 regarding State protection of human remains and funerary objects.

I. Composite

A Composite Map is provided as *Exhibit IV.I*. This map indicates the Property's existing physical constraints, including structures, topography, riparian habitat, utilities and easements.

Thornydale Storage Specific Plan

Exhibit IV.H: Arizona State Museum Records Check



THE UNIVERSITY OF ARIZONA
**ARIZONA
STATE MUSEUM**

Arizona State Museum
PO Box 210026
Tucson AZ 85721-0026
(520) 621-6281
www.statemuseum.arizona.edu

Robin Large
Lazarus & Silvyn, P.C.
5983 E. Grant Rd., Ste. 290
Tucson, AZ 85712

April 20, 2022

RE: Thornydale Storage Specific Plan
Parcel 225-02-027M

Dear Robin,

The Arizona State Museum (ASM) has reviewed archaeological project and site records in support of the following project:

Lazarus & Silvyn's Thornydale Storage Specific Plan project (Lazarus & Silvyn Project No. 2205-003;
ASM Job No. 004561)

Correspondence indicates this project will involve the rezoning of privately-owned land with the intent of developing a personal storage project. The project area is located at North Thornydale Road and West Overton Road within unincorporated Pima County, and encompasses parcel 225-02-027M within Township 12 South, Range 13 East, Section 20.

I invite you to review the results of ASM's research, which are summarized below.

Search Results:

According to a search of the archaeological site records and reports held in ASM collections, 41 archaeological investigations were conducted within a one-mile radius of the project area between 1975 and 2018. Of these 41 archaeological investigations, two intersect the project area.

For the two archaeological investigations that intersect the project area, Table 1 summarizes their basic information and scope.

Additionally, three archaeological sites have been identified within a one-mile radius of the project area. Of these three archaeological sites, none intersect the project area.

Page 1 of 2

Thornydale Storage Specific Plan

ASM Reference Number (AZProj/Accession)	Report Author(s)	Year(s) Conducted	Scope of Project
1981-0174	Madsen et al.	1981-1986	Archaeological survey for research
1988-0200	Heuett	1988	Archaeological survey for transmission line

Table 1. ASM archaeological investigations that intersect the project area

Recommendations and Responsibilities:

1. Although the entire project area has been previously surveyed, the work was conducted 34 to 41 years ago. It is standard archaeological practice for a property to be re-surveyed if the previous survey was conducted 10 or more years ago, as there is a possibility for previously unidentified archaeological sites to have since been exposed. Therefore, ASM recommends—but does not require—that a qualified archaeological contractor be consulted before any ground-disturbing activity begins. A list of archaeological contractors is available on the ASM website at:

<https://statemuseum.arizona.edu/crm/document/aaa-qualified-consultants>

2. Pursuant to Arizona Revised Statute §41-865, if any human remains or funerary objects are discovered during project work, all work must stop within the area of the remains and the ASM Repatriation Office must be contacted at 520-626-0320.

3. City, county, or municipal governments may have their own requirements; therefore, ASM recommends that the relevant jurisdiction(s) be consulted.

If you have any questions about the results of this records search, please feel free to contact me at jknightonwisor@arizona.edu or 520-621-4011.

Sincerely,



Jonathan Knighton-Wisor
Research Specialist
Archaeological Records Office
Arizona State Museum
520-621-4011
jknightonwisor@arizona.edu

References:

Heuett, Mary Lou

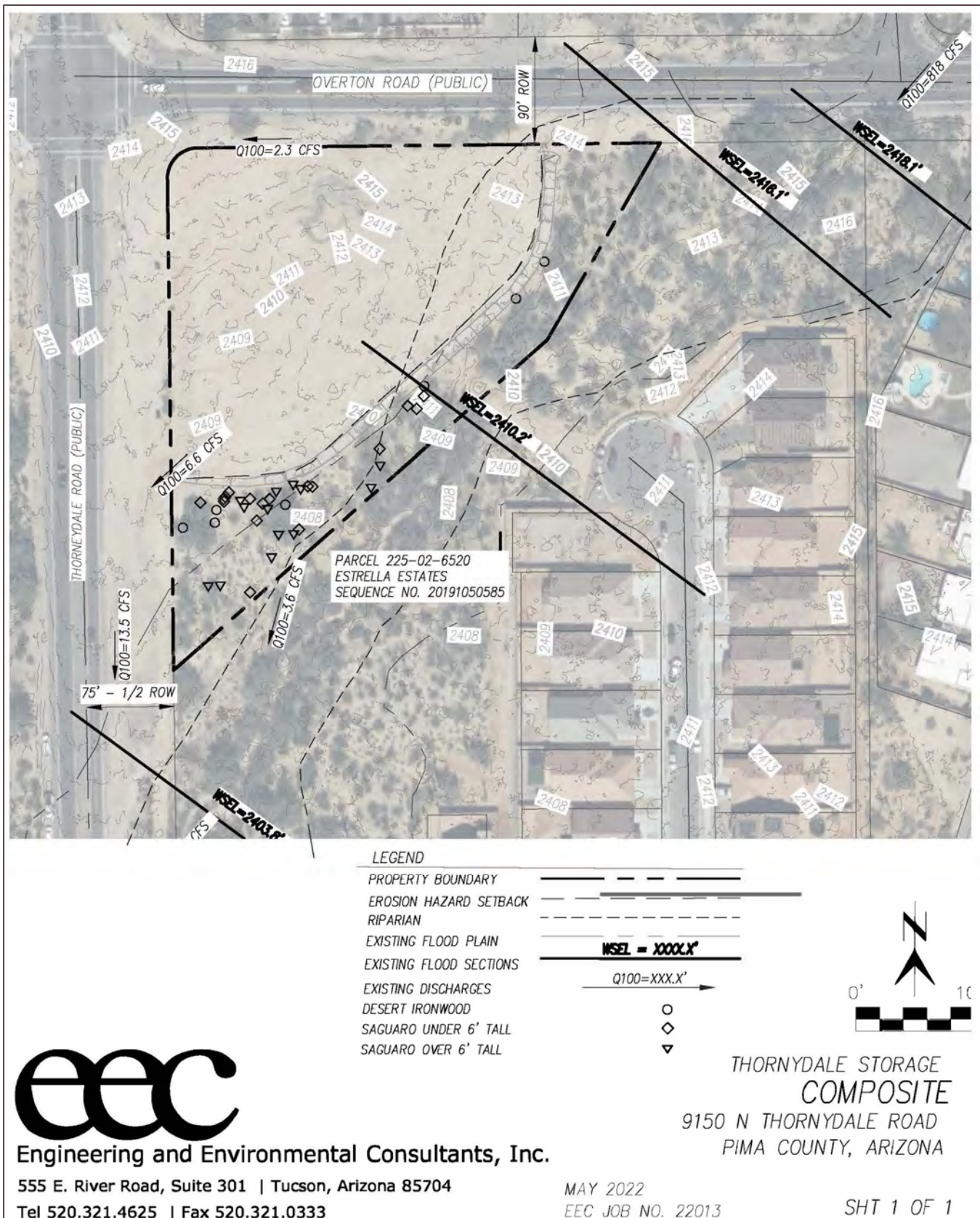
1988 *An Archaeological Survey of 3.1 Miles of a Right-of-Way for CAP Northwest Phase III Along Cortaro Farms, Thornydale, and Oasis Roads.*

Madsen, John, Paul Fish, and Suzanne Fish

1993 *The Northern Tucson Basin Survey: Research Directions and Background Studies.* Arizona State Museum Archaeological Series No. 182. Tucson, Arizona.

Thornydale Storage Specific Plan

Exhibit IV.I: Composite Map



Thornycastle Storage Specific Plan

V. CONDITIONS OF APPROVAL

A. Purpose

B. Board of Supervisors Conditions of Approval

Thornysdale Storage Specific Plan

VI. REFERENCES

Pima County. “*PimaMaps*.” Pima County, Arizona, <http://gis.pima.gov/pimamaps/>.

Pima County. “*Pima Prospers*.” Pima County, Arizona, adopted May 19, 2015.

Pima County. “*Title 18 – Zoning*.” Pima County, Arizona, Municipal Code, [http://library.amlegal.com/nxt/gateway.dll/Arizona/pimacounty_az/title18zoning?f=templates\\$fn=default.htm\\$3.0\\$vid=amlegal:pimacounty_az](http://library.amlegal.com/nxt/gateway.dll/Arizona/pimacounty_az/title18zoning?f=templates$fn=default.htm$3.0$vid=amlegal:pimacounty_az).

Appendix A

ALTA Survey

THIS SURVEY IS BASED ON THE TITLE REPORT ISSUED BY FIRST NATIONAL TITLE INSURANCE COMPANY, A TEXAS CORPORATION ("COMPANY"), FILE NO. 504-245401 JC, EFFECTIVE NOVEMBER 9, 2021 AT 7:30 AM.

EXHIBIT A LEGAL DESCRIPTION – PER TITLE REPORT

THE LAND REFERRED TO HEREIN BELOW IS SITUATED IN THE COUNTY OF PIMA, STATE OF ARIZONA, AND IS DESCRIBED AS FOLLOWS:

THE NORTH HALF OF THE WEST HALF OF THE NORTHWEST QUARTER OF THE SOUTHWEST QUARTER OF SECTION 20, TOWNSHIP 12 SOUTH, RANGE 13 EAST, GILA AND SALT RIVER BASE AND MERIDIAN, PIMA COUNTY, ARIZONA.

EXCEPT THE WEST 30 FEET THEREOF. AND

EXCEPT THAT PORTION AS CONVEYED TO PIMA COUNTY, A POLITICAL SUBDIVISION OF THE STATE OF ARIZONA BY WARRANTY DEED RECORDED IN DOCKET 9478, PAGE 43,

EXCEPT THAT PORTION LYING WITHIN IRONWOOD MEADOWS A SUBDIVISION OF PIMA COUNTY, ARIZONA, ACCORDING TO THE PLAT OF RECORD IN THE OFFICE OF THE PIMA COUNTY RECORDER IN BOOK 42 OF MAPS AND PLATS AT PAGE 16.

EXCEPT THAT PORTION AS CONVEYED TO PIMA COUNTY, A POLITICAL SUBDIVISION OF THE STATE OF ARIZONA BY WARRANTY DEED RECORDED IN SEQUENCE NO. 20181510661

EXCEPT THAT PORTION LYING WITHIN THE FINAL PLAT FOR ESTRELLA ESTATES A SUBDIVISION OF PIMA COUNTY, ARIZONA, ACCORDING TO THE PLAT OF RECORD IN THE OFFICE OF THE PIMA COUNTY RECORDER RECORDED IN SEQUENCE NO. 20191050584

EXCEPT ALL COAL AND OTHER MINERALS A RESERVED IN PATENT FROM THE UNITED STATES OF AMERICA.

THIS SURVEY IS BASED ON THE TITLE REPORT ISSUED BY FIRST NATIONAL TITLE INSURANCE COMPANY, A TEXAS CORPORATION ("COMPANY"), FILE NO. 504-245401 JC, EFFECTIVE NOVEMBER 9, 2021 AT 7:30 AM.

SCHEDULE B, PART II: EXCEPTIONS PER TITLE REPORT

THIS COMMITMENT DOES NOT REPUBLISH ANY COVENANT, CONDITION, RESTRICTION, OR LIMITATION CONTAINED IN ANY DOCUMENT REFERRED TO IN THIS COMMITMENT TO THE EXTENT THAT THE SPECIFIC COVENANT, CONDITION, RESTRICTION, OR LIMITATION VIOLATES STATE OR FEDERAL LAW BASED ON RACE, COLOR, RELIGION, SEX, SEXUAL ORIENTATION, GENDER IDENTITY, HANDICAP, FAMILIAL STATUS, OR NATIONAL ORIGIN.

THE POLICY WILL NOT INSURE AGAINST LOSS OR DAMAGE RESULTING FROM THE TERMS AND PROVISIONS OF ANY LEASE OR EASEMENT IDENTIFIED IN SCHEDULE A, AND WILL INCLUDE THE FOLLOWING EXCEPTIONS UNLESS CLEARED TO THE SATISFACTION OF THE COMPANY:

1. ANY DEFECT, LIEN, ENCUMBRANCE, ADVERSE CLAIM, OR OTHER MATTER THAT APPEARS FOR THE FIRST TIME IN THE PUBLIC RECORDS OR IS CREATED, ATTACHES, OR IS DISCLOSED BETWEEN THE COMMITMENT DATE AND THE DATE ON WHICH ALL OF THE SCHEDULE B, PART I--REQUIREMENTS ARE MET.
2. (A) TAXES OR ASSESSMENTS THAT ARE NOT SHOWN AS EXISTING LIENS BY THE RECORDS OF ANY TAXING AUTHORITY THAT LEVIES TAXES OR ASSESSMENTS ON REAL PROPERTY OR BY THE PUBLIC RECORDS; (B) PROCEEDINGS BY A PUBLIC AGENCY THAT MAY RESULT IN TAXES OR ASSESSMENTS, OR NOTICES OF SUCH PROCEEDINGS, WHETHER OR NOT SHOWN BY THE RECORDS OF SUCH AGENCY OR BY THE PUBLIC RECORDS.
3. ANY FACTS, RIGHTS, INTERESTS OR CLAIMS THAT ARE NOT SHOWN BY THE PUBLIC RECORDS BUT THAT COULD BE ASCERTAINED BY AN INSPECTION OF THE LAND OR THAT MAY BE ASSERTED BY PERSONS IN POSSESSION OF THE LAND.
4. EASEMENTS, LIENS OR ENCUMBRANCES, OR CLAIMS THEREOF, NOT SHOWN BY THE PUBLIC RECORDS.
5. ANY ENCROACHMENT, ENCUMBRANCE, VIOLATION, VARIATION, OR ADVERSE CIRCUMSTANCE AFFECTING THE TITLE THAT WOULD BE DISCLOSED BY AN ACCURATE AND COMPLETE LAND SURVEY OF THE LAND AND NOT SHOWN BY THE PUBLIC RECORDS.
6. (A) UNPATENTED CLAIMS; (B) RESERVATIONS OR EXCEPTIONS IN PATENTS OR IN ACTS AUTHORIZING THE ISSUANCE THEREOF; (C) WATER RIGHTS, CLAIMS OR TITLE TO WATER; WHETHER OR NOT THE MATTERS EXCEPTED UNDER (A), (B), OR (C) ARE SHOWN BY THE PUBLIC RECORDS.
7. ANY LIEN, OR RIGHT TO A LIEN, FOR SERVICES, LABOR OR MATERIAL HERETOFORE OR HEREAFTER FURNISHED, IMPOSED BY LAW AND NOT SHOWN BY THE PUBLIC RECORDS.
8. DEFECTS, LIENS, ENCUMBRANCES, ADVERSE CLAIMS OR OTHER MATTERS, IF ANY, CREATED, FIRST APPEARING IN THE PUBLIC RECORDS OR ATTACHING TO THE SUBSEQUENT EFFECTIVE DATE HEREOF BUT PRIOR TO THE DATE THE PROPOSED INSURED ACQUIRES OF RECORDS FOR VALUE THE ESTATE OR INTEREST OR MORTGAGE THEREON COVERED BY THIS PUBLIC.

SCHEDULE B, PART II: EXCEPTIONS PER TITLE REPORT (CONTINUATION)

(NOTE: THE ABOVE EXCEPTIONS NOS. 2 THROUGH 8, INCLUSIVE, WILL BE ELIMINATED FROM ANY A.L.T.A. EXTENDED COVERAGE POLICY, A.L.T.A. HOMEOWNER'S POLICY, A.L.T.A. EXPANDED COVERAGE RESIDENTIAL LOAN POLICY AND ANY SHORT FORM VERSIONS THEREOF. HOWEVER, THE SAME OR SIMILAR EXCEPTION MAY BE MADE IN SCHEDULE B OF THOSE POLICIES IN CONFORMITY WITH THE REMAINING EXCEPTIONS OF THIS COMMITMENT SHOWN BELOW.)

9. TAXES FOR THE SECOND HALF OF 2021, A LIEN, NOT YET DUE.
10. ANY ACTION BY THE COUNTY ASSESSOR AND/OR TREASURER, ALTERING THE CURRENT OR PRIOR TAX ASSESSMENT, SUBSEQUENT TO THE DATE OF THE POLICY OF TITLE INSURANCE.
11. RESERVATIONS OR EXCEPTIONS IN PATENTS OR IN ACTS AUTHORIZING THE ISSUANCE THEREOF.
12. WATER RIGHTS, CLAIMS OR TITLE TO WATER, AND AGREEMENTS, COVENANTS, CONDITIONS OR RIGHTS INCIDENT THERETO, WHETHER OR NOT SHOWN BY THE PUBLIC RECORDS. THIS EXCEPTION IS NOT LIMITED BY REASON OF THE DISCLOSURE OF ANY MATTER RELATING TO WATER RIGHTS AS MAY BE SET FORTH ELSEWHERE IN SCHEDULE B.
13. LIABILITIES AND OBLIGATIONS IMPOSED UPON SAID LAND BY ITS INCLUSION WITHIN ANY LEGALLY FORMED DISTRICTS.
14. ESTABLISHED AND/OR EXISTING ROADS, HIGHWAYS, RIGHTS-OF-WAY OR EASEMENTS.
15. EASEMENT(S) FOR THE PURPOSES SET FORTH THEREIN AND RIGHTS INCIDENT THERETO AS SET FORTH IN BOOK 75 OF MISCELLANEOUS RECORDS AT PAGE 66.
16. RESTRICTION AS SET FORTH IN DEED RECORDED IN DOCET 1687 AT PAGE 441
17. EASEMENT(S) FOR THE PURPOSES SET FORTH THEREIN AND RIGHTS INCIDENT THERETO AS SET FORTH IN DOCKET 9456 AT PAGE 1771. {PLOTTED}
18. TERMS, CONDITIONS, RESTRICTIONS, EASEMENTS, LIABILITIES AND/OR OBLIGATIONS AS SET FORTH IN RESOLUTION 2017-4RECORDED IN SEQUENCE NO.: 20170240760 {AFFECTS SUBJECT PARCEL}
19. MATTERS SHOWN ON SURVEY RECORDED IN SEQUENCE NOS. 20173490074 AND 20182050009
20. TERMS, CONDITIONS, RESTRICTIONS, EASEMENTS, LIABILITIES AND/OR OBLIGATIONS AS SET FORTH IN ORDINANCE 2018-24 RECORDED IN SEQUENCE NO.: 20182260377 {AFFECTS SUBJECT PARCEL}
21. EASEMENT(S) FOR THE PURPOSES SET FORTH THEREIN AND RIGHTS INCIDENT THERETO AS SET FORTH IN SEQUENCE NO.: 20190660253 {PLOTTED}
22. EASEMENT(S) FOR THE PURPOSES SET FORTH THEREIN AND RIGHTS INCIDENT THERETO AS SET FORTH IN SEQUENCE NO.: 20191140105 {PLOTTED}

SHEET INDEX

SHEET	DESCRIPTION
1	COVER SHEET: ALTA/NSPS LAND TITLE SURVEY
2	RECORD OF SURVEY

LEGEND

(C) CALCULATED DIMENSION.	OHE OVERHEAD ELECTRIC
(M) MEASURED DIMENSION PER THIS SURVEY.	PP POWER POLE
(R) RECORD DIMENSION PER MAPS & PLATS.	SSMH SANITARY SEWER MANHOLE
☐ FND BRASS CAP SURVEY MONUMENT (BCSM).	TSB TRAFFIC SIGNAL BOX
○ SET 1/2" IRON PIN, TAGGED 'RLS 12537'.	TSL TRAFFIC SIGNAL LIGHT
● FOUND AS NOTED.	USWMH UW WEST MANHOLE
CLB CENTURY LINK BOX	WTW WATER TEST WELL
FOMKR FIBER OPTIC MARKER	WV WATER VALVE
GMKR GAS MARKER	
GP GAS PIPE	
GMB GAS METER BOX	
GR GAS RISER	

REFERENCE:
JN 17174 ALTA/NSPS PARCELS 1-4 KB HOMES SURVEY DRAWINGS.

RECORD OF SURVEY SEQUENCE NO. 20182050009

GENERAL NOTES:

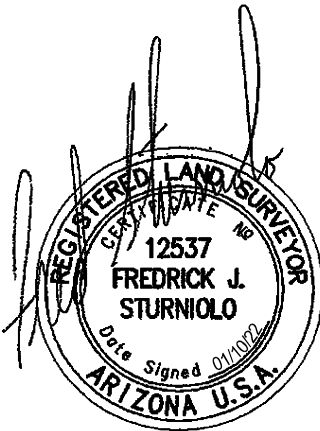
- BASIS OF BEARINGS: WEST LINE OF THE SW¹/₄ OF SECTION 20.
SAID BEARING BEING: S 00°30'27" E
1. PARCEL 1: AREA = +102,573 SQ. FT. = +2.3548 ACRES
2. { } SURVEYORS COMMENTS.
3. ZONING: CB-1 LOCAL BUSINESS ZONE (71.76% OR 1.72 ACRES)
CR-5 MULTIPLE RESIDENCE ZONE (28.24% OR 0.68 ACRES)

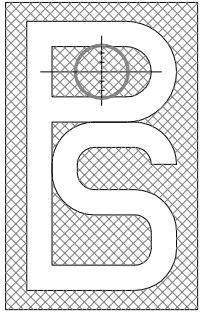
DISCLAIMER:
THE WORD "CERTIFY" OR "CERTIFICATE" IS AN EXPRESSION OF PROFESSIONAL OPINION REGARDING THE FACTS OF THE SURVEY AND DOES NOT CONSTITUTE A WARRANTY OR GUARANTEE, EXPRESSED OR IMPLIED.

SURVEYOR'S CERTIFICATE:
THE UNDERSIGNED, BEING A REGISTERED LAND SURVEYOR IN THE STATE OF ARIZONA, CERTIFIES TO FIRST NATIONAL TITLE INSURANCE COMPANY, A TEXAS CORPORATION ("COMPANY") AND ITS SUCCESSORS AND WONG FAMILY LIMITED PARTNERSHIP, AN ARIZONA LIMITED PARTNERSHIP AND ITS SUCCESSORS AS FOLLOWS: THIS IS TO CERTIFY THAT THIS MAP OR PLAT AND THE SURVEY ON WHICH IT IS BASED WERE MADE IN ACCORDANCE WITH THE FEBRUARY 23, 2016 MINIMUM STANDARD DETAIL REQUIREMENTS FOR ALTA/NSPS LAND TITLE SURVEYS, JOINTLY ESTABLISHED AND ADOPTED BY ALTA AND NSPS, AND INCLUDES ITEMS 1, 4, 8, AND 11 OF TABLE A THEREOF. THE FIELD WORK WAS COMPLETED IN JANUARY 04, 2022.

DATE:

FREDRICK J. STURNIOLO, RLS
REGISTRATION No. 12537



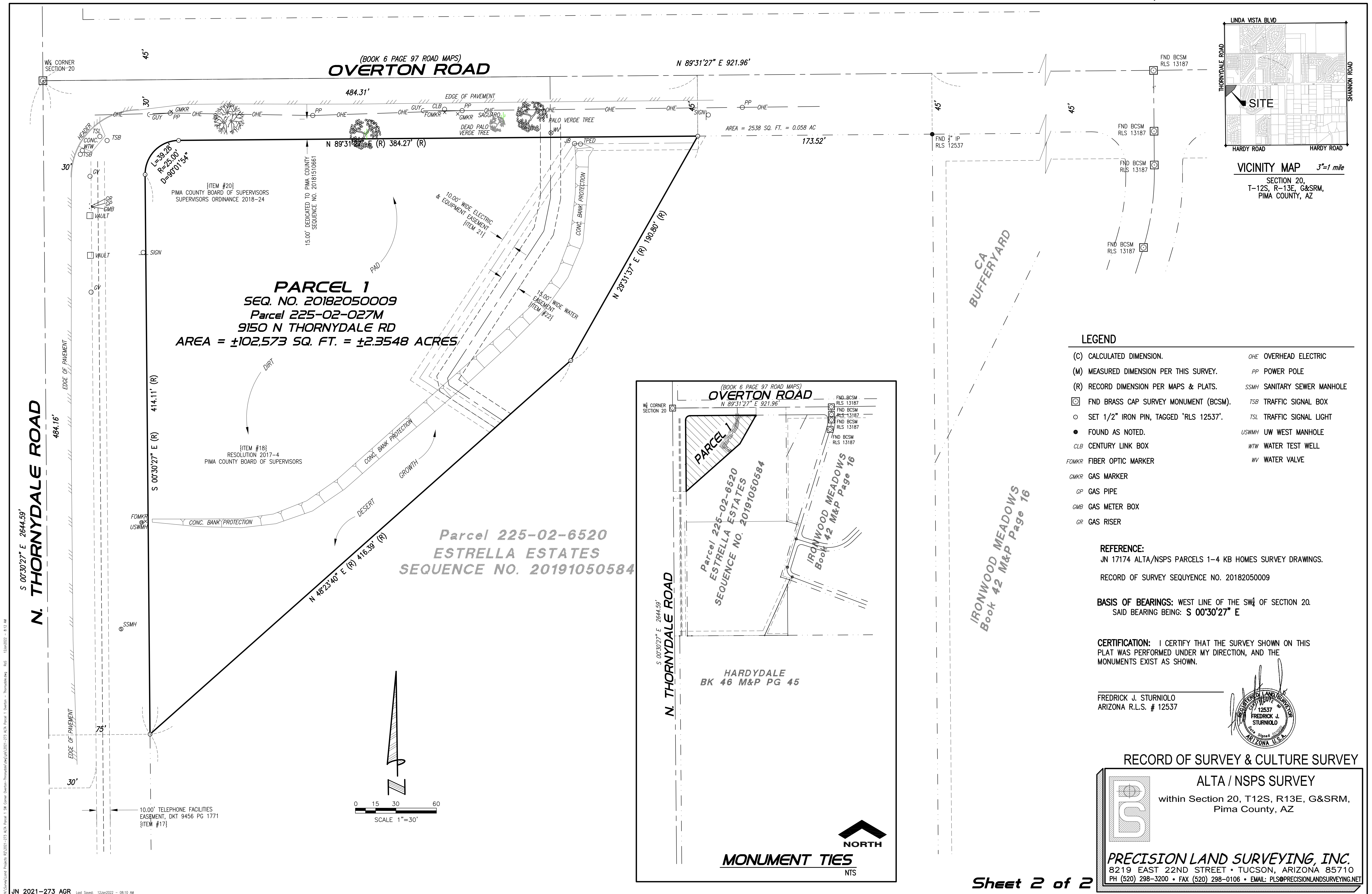


ALTA / NSPS SURVEY
within Section 20, T12S, R13E, G&SRM,
Pima County, AZ

PRECISION LAND SURVEYING, INC.
8219 EAST 22ND STREET • TUCSON, ARIZONA 85710
PH (520) 298-3200 • FAX (520) 298-0106 • EMAIL: PLS@PRECISIONLANDSURVEYING.NET

9150 N THORNYDALE RD

Sheet 1 of 2



Appendix B

Biological Impact Report



Biological Impact Report Thornydale and Overton

NE#22039

**Prepared for:
Mr. Zach Channing
ZDC Properties, LLC
18381 Long Lake Drive
Boca Raton, FL 33496**

**Prepared by:
Novak Environmental, Inc.
4574 N. 1st Avenue
Tucson, Arizona 85718
(520) 206-0591**

**For questions regarding this report please contact: Karen Cesare, RLA
karen@novakenvironmental.com**

TABLE OF CONTENTS

I. INTRODUCTION	3
<i>Figure 1. Location Map.....</i>	<i>3</i>
II. LANDSCAPE RESOURCES	4
III. SPECIES-SPECIFIC INFORMATION.....	5
IV. SAGUAROS AND IRONWOODS.....	7
V. SUMMARY	7



BIOLOGICAL IMPACT REPORT

Thornydale and Overton Rezoning

May 10, 2022

I. INTRODUCTION

This Biological Impact Report is for an approximately 2.4-acre site on the southeast corner of N. Thornydale Road and W. Overton Road in Pima County, Arizona, parcel number 225-02-027M. The owner is seeking to rezone the property through a Specific Plan. The property is in Sections 20, T12S, R13E, G. & S.R.M., Pima County, Arizona (see Figure 1).

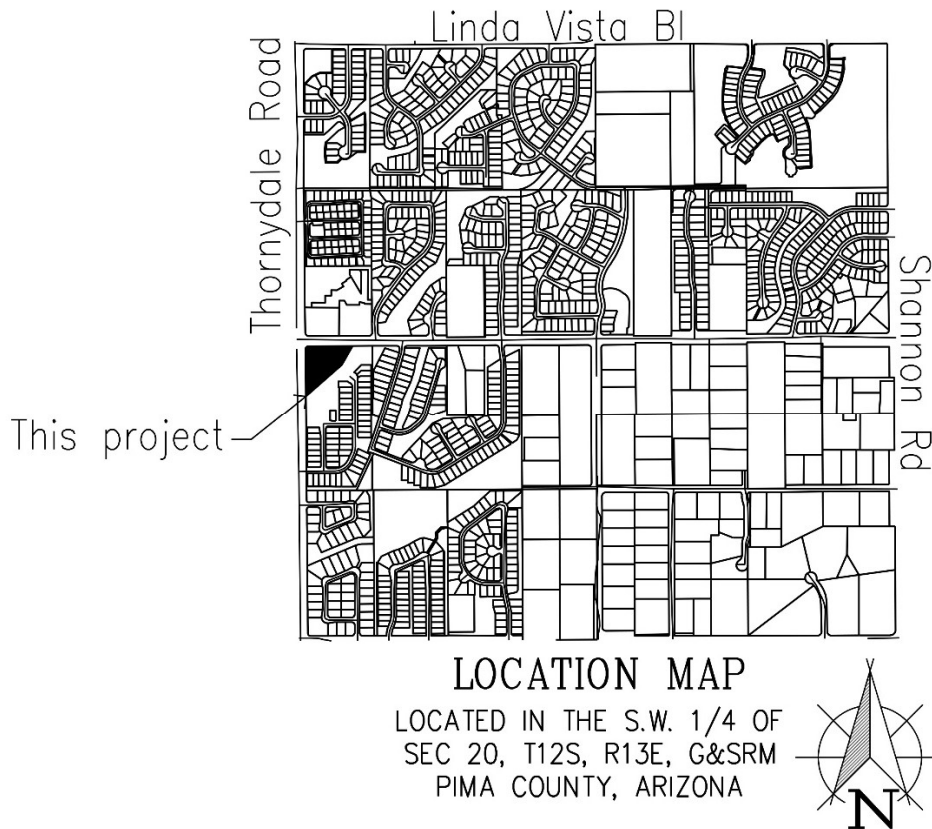


Figure 1. Location Map
1 Parcels (225-02-027M)
Section 20, T12S, R13E, G. & S.R.M., Pima County, Arizona

This required Biological Impact Report will present responses, as they pertain to the subject property, to all questions set forth in the Pima County Development Services Biological Impact Report Guidelines, March 2010.

II. LANDSCAPE RESOURCES

1. Identify whether the proposed site occurs wholly or partially within any Maeveen Marie Behan Conservation Lands System Category including Important Riparian Areas and Special Species Management Areas.

Yes, the site is located in CLS Category Special Species Management Area, and the northeast portion of the site is partially within the CLS Category Important Riparian Area Xeroriparian C.

However, a prior rezoning and approved development plan fully addressed the CLS and provided the required mitigation. This site has been cleared, except for a small area on the south and eastern edge, east of the existing bank protection. The currently undisturbed areas will remain undisturbed.

2. Identify whether the proposed project occurs in the vicinity of any of the six general areas identified as Critical Landscape Linkages.

This project occurs to the southwest of Critical Landscape Linkage area number 1, and southeast of Critical Landscape Linkage area number 2.

3. If the property is a Habitat Protection or Community Open Space priority acquisition property, as displayed on SDCP MapGuide, identify which designation applies to the site and comment on

the status of communications, if any, between the owner and Pima County regarding the County's potential acquisition of the property.

This site is not included as a priority acquisition for either Habitat Protection or Community Open Space.

III. SPECIES-SPECIFIC INFORMATION (including Pertinent Federally-Threatened and Endangered Species)

Cactus Ferruginous Pygmy-owl:

1. Does the proposed project site occur within Survey Zone 1 for the cactus ferruginous pygmy-owl?

Yes, it is within the Pygmy-Owl Survey Zone 1.

2. Has the proposed project site been surveyed for pygmy-owls?

a. If yes, disclose the dates when surveys were done and provide a summary of the results.

b. If no, are surveys planned in the future?

No. The project site has not been surveyed for pygmy-owls; there are no surveys planned in the future.

The site has been mostly cleared of vegetation.

Western Burrowing Owl:

1. Does the proposed project site occur within the Priority Conservation Area for the Western Burrowing Owl?

No.

2. Has the proposed project site been surveyed for burrowing owls?

a. If yes, disclose the dates when surveys were done and provide a summary of the results.

b. If no, are surveys planned in the future?

No. The project site has not been surveyed for Western Burrowing Owls; there are no surveys planned in the future.

Pima Pineapple Cactus

1. Does the proposed project site occur within the Priority Conservation Area for the Pima pineapple cactus?

No.

2. Have Pima pineapple cactus been found on the proposed project site?

No. No Pima pineapple cacti have been found on the project site.

3. Has the proposed project site been surveyed for Pima pineapple cactus?

a. If yes, disclose the date when surveys were done and provide a summary of the results.

b. If no, are surveys planned in the future?

No. The project site has not been surveyed for Pima pineapple cactus; no surveys are planned in the future.

Needle-Spined Pineapple Cactus:

1. Does the proposed project site occur within the Priority Conservation Area for the needle-spined pineapple cactus?

No.

2. Have needle-spined pineapple cactus been found on the proposed project site?

No needle-spined pineapple cactus have been found on the project site.

3. Has the proposed project site been surveyed for needle-spined pineapple cactus?

a. If yes, disclose the date when surveys were done and provide a summary of the results.

b. If no, are surveys planned in the future?

No. The project site has not been surveyed for needle-spined pineapple cactus; no surveys are planned in the future.

IV. SAGUAROS AND IRONWOODS

While the vast majority of the site has been previously cleared, a small area on the south portion of the property and the east side of the property contains undisturbed natural open space. These areas are outside of the cleared development pad, which is protected by existing concrete bank protection.

Within the undisturbed areas there are 30 saguaros, 17 are 6-feet or under and 13 are over 6 feet, with one large saguaro that is twenty feet tall. There are also 8 ironwood trees. All these plants will remain “preserved-in-place”. See the “Constraints Composite” map for plant locations.

V. SUMMARY

This report presents a Biological Impact Report for the Thornydale and Overton Rezoning, an approximately 2.4-acre parcel located in Pima County. This Biological Impact Report, required as part of the Specific Plan request, presents responses to all questions set forth in the Pima County Development Services Biological Impact Report Guidelines, March 2010. The findings indicate that while this site is within areas of concern included in the report guidelines, the site has been previously cleared as part of an approved development plan and any CLS mitigation for this site has been completed.

Appendix C

Traffic Impact Analysis

SEC Thornydale-Overton Self-Storage

Traffic Memorandum

Prepared for submittal to:

Pima County, AZ

Prepared by:

M Esparza
Engineering, LLC

M Esparza Engineering, LLC
2934 W. Salvia Drive
Tucson, AZ 85745

June 13, 2022

SEC Thornydale-Overton Self-Storage Traffic Memorandum

Prepared for submittal to:

Pima County, Arizona

Prepared by:

M Esparza Engineering, LLC
2934 W. Salvia Drive
Tucson, AZ 85745

Phone: (520) 207-3358
Project No. 2022.15
Marcos Esparza, P.E., Principal



June 13, 2022

This study has been prepared using available traffic data and forecasts, as well as limited field data collected specifically for this study. It is intended for use in making a determination regarding the transportation infrastructure needs of the study area. It does not represent a standard or specification. The document is copyrighted by Pima County and M Esparza Engineering, LLC, 2934 W. Salvia Drive, Tucson, AZ 85745, telephone 520-207-3358. All rights are reserved pursuant to United States copyright law. The document may not be reproduced digitally or mechanically, in whole or in part, without the prior written approval of M Esparza Engineering, LLC, except as noted in the following. (1) Limited quotations may be made, for technical purposes only, as long as proper citation to the authors is provided. (2) Governmental agencies to which this report is submitted for review may make limited copies for internal use and to fulfill public requests under the Freedom of Information Act.

Table of Contents

1.	INTRODUCTION AND SUMMARY	1
	Purpose of Report and Study Objectives	1
	Executive Summary	1
2.	PROPOSED DEVELOPMENT	5
	Site Location.....	5
	Land Use and Intensity	5
	Site Plan.....	5
	Development Phasing and Timing.....	5
3.	STUDY AREA CONDITIONS	6
	Study Area and Horizon Year	6
	Land Use.....	6
	Site Accessibility.....	6
4.	ANALYSIS OF EXISTING CONDITIONS	7
	Physical Characteristics	7
5.	EXISTING AND PROJECTED TRAFFIC.....	10
	Traffic Volumes	10
	Site Traffic Forecasting	10
	Site Traffic Forecasting	11
	Auxiliary Lane Warrants	12
	Crash Analysis	12
6.	CONCLUSIONS AND RECOMMENDATIONS	15

List of Exhibits

Exhibit 1	Site Plan	2
Exhibit 2	Site Location	3
Exhibit 3	Roadway Inventory.....	8
Exhibit 4	Thornsdale/Overton	8
Exhibit 5	Trip Generation	10
Exhibit 6	Trip Distribution.....	11
Exhibit 7	2023 Roadway Daily Volumes	12
Exhibit 8	Pima County Right Turn Warrants on Two-Lane Roads	13
Exhibit 9	Collision History - Intersections.....	13
Exhibit 10	Collision History - Roadways.....	14

1. Introduction and Summary

Purpose of Report and Study Objectives

This traffic memorandum (TM) addresses traffic operations and roadway design for a proposed self-storage east of Thornydale Road and south of Overton Road. The project is in unincorporated Pima County. The current zoning is CB-1. This TM is provided to support a rezoning application to revise the zoning to Specific Plan.

The project is a three-story 114,800 square foot indoor self-storage building. Exhibit 1 shows the conceptual site plan. Exhibit 2 shows the site location.

The objectives of this study are to determine the traffic impacts of the proposed development on the adjacent roadway system and to recommend any needed improvements to maintain efficient and safe traffic operations. The specific study objectives are as follows:

- Evaluate the impact at the project driveways.
- Evaluate the roadways Thornydale Road and Overton Road adjacent to the project and recommend any needed improvements.
- Evaluate the appropriateness of the proposed driveway locations.

The project will generate an estimated 166 daily trips with 10 AM peak hour trips and 17 PM peak hour trips. Based on the projected trip generation, this report includes the required analysis for a Traffic Memorandum. This report analyzes existing, future “without project” and future “with project” conditions at Thornydale Road/Overton Road and the site access drives. The analysis for the future year conditions estimates the impacts in 2023, the assumed opening year.

Executive Summary

Site Location and Study Area

The project site is east of Thornydale Road and south of Overton Road. The project is in unincorporated Pima County. The site location is shown in Exhibit 2. The study area includes the roadways along the frontage of the project and the access locations.

Development Description

The site plan of the proposed development includes a 114,800 square foot self-storage building. Access to the site is proposed from Thornydale Road and Overton Road.

The north driveway on Overton Road would be located opposite the driveway to the Thornydale Village shopping center. The south driveway will be 270 feet from Overton Road.

The spacing of project driveways will meet Pima County driveway spacing and corner clearance guidelines as defined in the Pima County Subdivision and Development Street Standards.

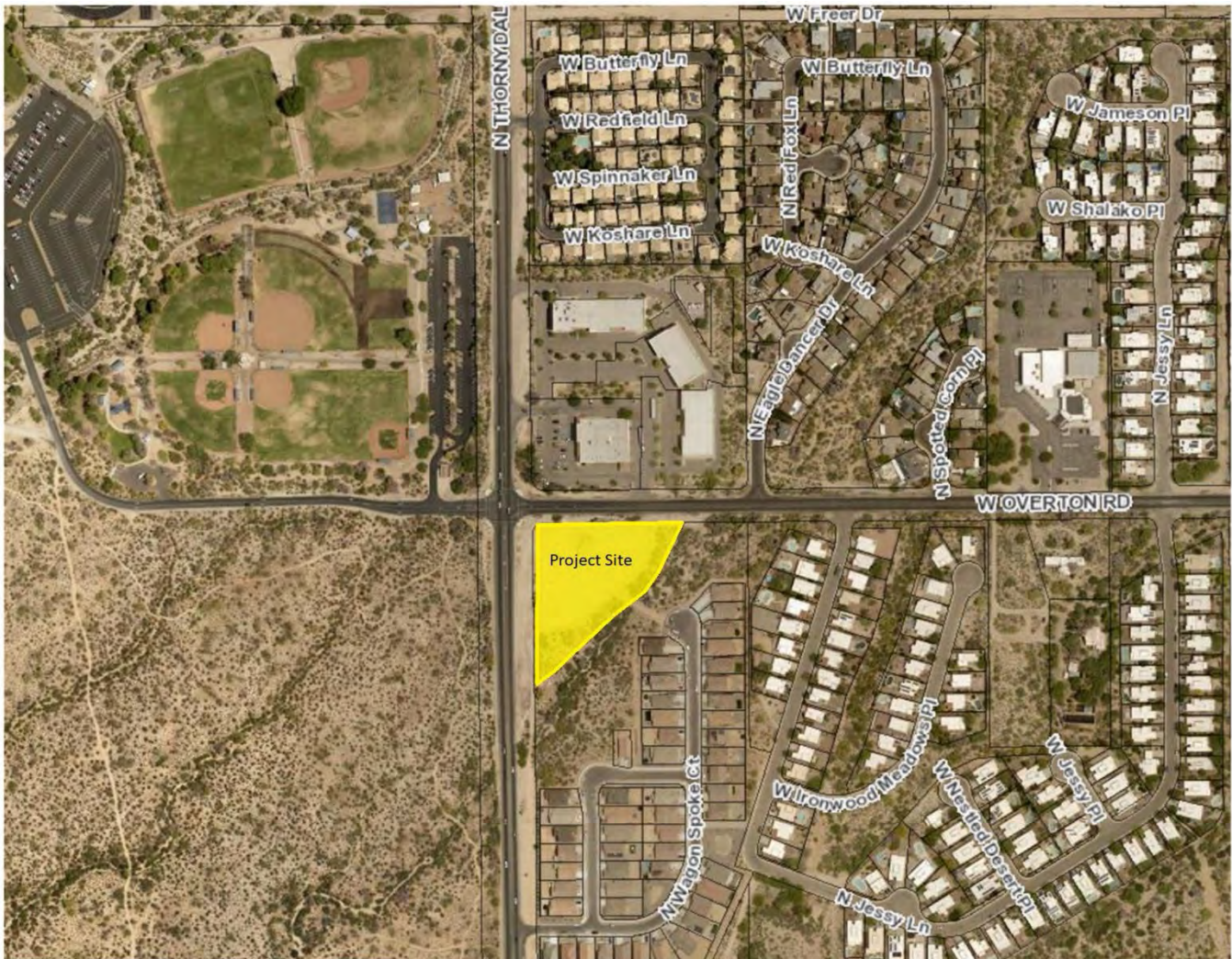
Based on trip rates for “mini-warehouse” from the Institute of Transportation Engineering (ITE) *Trip Generation Manual 11th Edition*, the project will add approximately 166 daily trips with 10 AM peak hour trips and 17 PM peak hour trips to the roadway system.

Exhibit 1 Site Plan



Source: RKAAR Architects

Exhibit 2 Site Location



Summary of Conclusions and Recommendations

The following is a summary of conclusions and recommendations for the roadways within and surrounding the project site.

Conclusions

1. The project will generate an estimated 166 daily trips with 10 AM peak hour trips and 17 PM peak hour trips. These trips will be distributed to and from the project site at two driveways.
2. The driveway spacings and corner clearances will meet Pima County minimum spacing standards.
3. Based on criteria in the *Pima County Subdivision and Development Street Standards*, turn lanes are not warranted at the project driveways.

Recommendations

1. Construct the project driveways to Pima County standards, with one ingress and one egress lane.
2. Remove or trim vegetation at the Overton driveway location to ensure that there is acceptable sight distance to and from the project entrance.
3. Discuss with Pima County whether the project driveway on Thornydale Road should be restricted to rights in and out and left out due to the proximity of the Thornydale Road northbound left turn lane at Overton Road.
4. Roadway and subdivision design should conform to current Pima County standards.
5. All new traffic signs and markings must comply with the current *Manual on Uniform Traffic Control Devices* and local requirements.

2. Proposed Development

Site Location

The project site is east of Thornydale Road and south of Overton Road. The project is in unincorporated Pima County. The site location is shown in Exhibit 2.

Land Use and Intensity

The project includes 114,800 square feet of self-storage use in a three-story building.

Site Plan

The site plan shows a small office at the southwest end of the building and ten parking spaces south of the building. A thirty-foot drive provides access from Overton Road and from Thornydale Road to the project.

Access Geometrics

The driveways should be constructed to Pima County standards. It is recommended that the driveways provide one entering lane and one exiting lane and that the driveway legs be controlled by a stop sign. All access locations will allow for full access, although Pima County may request the southern access to be left out, right in/out only to eliminate the potential for crashes associated with the proximity of the northbound left turn lane at Thornydale/Overton.

Development Phasing and Timing

For the purpose of this study, we have assumed a buildout year of 2023.

3. Study Area Conditions

Study Area and Horizon Year

The study area includes Thornydale Road, Overton Road and the project access locations. A Traffic Memorandum is appropriate for this project based on the projected trip generation. The horizon year is the opening year of the project, assumed to be 2023 for the purposes of this study.

Land Use

The project site is currently vacant and zoned CB-1. The Arthur Pack Regional Park is on the west side of Thornydale Road. The Thornydale Village shopping center is north of the project. The Estrella Estates and Ironwood Meadows neighborhoods are south and southeast of the project site. There are no known planned developments adjacent to the project site.

Site Accessibility

Access to the project will be via one driveway on Thornydale Road and one on Overton Road. All driveways will be at least 185 feet from the next driveway or will be directly opposite an existing one (North Driveway is opposite the Thornydale Village shopping center driveway).

The northbound turn lane on Thornydale Road is 165 feet long, and the corner clearance from Overton Road to the Thornydale project driveway is 270 feet long. There is a minor potential for conflicts between northbound left turning vehicles at the Thornydale Road/Overton Road intersection and southbound left turns into the project driveway. The potential is minor because the projected inbound volumes into the project driveway on Thornydale Road are minimal. Further discussion with Pima County staff is necessary on this issue.

Existing and Future Area Roadway System

Thornydale Road, Overton Road, Linda Vista Boulevard and Shannon Road will provide the primary regional access to the project site. They are all two-lane roadways, with both Thornydale Road and Overton Road having a two-way, left turn lane along the frontage of the property.

The Pima Association of Governments (PAG) FY 2022-2026 Transportation Improvement Program (TIP) does not have any projects that are approved for funding in its project list.

Site Circulation

Access will be via a thirty-foot lane that connects Thornydale Road to Overton Road.

4. Analysis of Existing Conditions

Physical Characteristics

Roadway Characteristics

Exhibit 3 is an inventory of the physical features of the project area roads. The following describes the roadway features of the study area roads.

Thornydale Road – is a nine-mile north/south paved roadway from its northern paved terminus north of Moore Road to its southern terminus at River Road. It is a two-lane arterial road with a two-way left turn lane in the vicinity of the project. It is classified as a medium volume arterial with a 150-foot right of way in the Pima County Major Streets Plan and as a Scenic, Major Route in the Pima County Scenic Routes Plan.

In the vicinity of the project, Thornydale Road has a 45-mph speed limit. It has unpaved shoulders along the project frontage. It is classified as a minor arterial. There are some sidewalks on the east side between Pecos Drive and Linda Vista Boulevard. There are no bike routes along the frontage of the project site.

Sun Shuttle Route 412 (Thornydale/River) runs along Thornydale Road with a stop at Thornydale/Linda Vista.

Overton Road is a two-lane east/west divided with a two-way left turn lane roadway that, on the east side of Thornydale Road, is classified as an urban major collector in ADOT's Functional Classification system. It is classified as a low volume arterial with a 90-foot right of way in the Pima County Major Streets Plan. On the west side of Thornydale Road, it continues as a two-lane undivided park access road into the Arthur Pack Regional Park.

Its speed limit is 40 mph. There are no sidewalks, bike lanes or bus routes along the project frontage.

Traffic Control Devices

Thornydale Road/Overton Road is the closest major intersection to the site. It is a four-leg signalized intersection. The southbound phase has a leading left turn phase, with all other approaches having permitted left turn phasing. Each approach has a left turn lane and a shared through/right turn lane. There are crosswalks on each leg of the intersection.

Exhibits 4 is an aerial photograph of the Thornydale Road/Overton Road intersection.

Stop signs are recommended for traffic control at the project driveways for drivers exiting the project. Sign construction and placement should comply with the MUTCD and local policies.

Traffic volumes at the project driveways will not warrant traffic signal control.

Exhibit 3 Roadway Inventory

Street	Weekday Daily Volume	Data Year	Source	ROW Width (ft)	No. Thru Lanes	Speed Limit	Sidewalks	Bike Route	Daily Capacity at LOS D*
Thornydale Road									
Linda Vista to Overton	19,514	2021	PAG	150	2	40	No	No	16,730
Overton to Hardy	16,956	2021	PAG	150	2	40	No	No	16,730
Overton Road									
West of Thornydale	1,356	2021	PAG	N/A	2	15	No	No	10,660
Thornydale Road to Shannon Road	7,810	2021	PAG	75-115	2	35	No	No	10,660

*Generalized Annual Average Daily Volumes for Florida's Urbanized Areas, from 2020 FDOT Quality/Level of Service Handbook Tables.

Exhibit 4 Thornydale/Overton



Transit Service

There is a Sun Shuttle route on Thornydale Road, with a stop at the southwest corner of Thornydale/Linda Vista. Besides this shuttle service, there is no fixed route service within the study area.

Pedestrian/Bicycle Facilities

With few exceptions, the roadways within the study area have unpaved shoulders with no sidewalks or bike lanes. The project will not have bicycle or pedestrian pathways as this project does not attract pedestrians or bicyclists based on the land use.

Sight Distance

A review of the sight distance at the project driveways found that vegetation at the Overton Road driveway location may need to be trimmed back or removed to ensure adequate sight distance. There do not appear to be any sight distance concerns at the Thornydale Road location.

5. Existing and Projected Traffic

Traffic Volumes

Daily traffic volumes for Thornydale Road and Overton Road in the vicinity of the study area are available on the Pima Association of Governments (PAG) website. The daily volumes are shown in Exhibit 3, Roadway Inventory.

Level of Service

Level of service is a qualitative description of how well a roadway or intersection operates under prevailing traffic conditions based on traffic volumes and capacity. A grading system of A through F, similar to academic grades, is utilized. LOS A is free-flowing traffic, whereas LOS F is forced flow and extreme congestion. LOS D is generally accepted as the standard in urbanized areas although LOS E is sometimes accepted in more congested areas. Segment performance has been estimated using the planning methods contained in the 2020 Florida Department of Transportation (FDOT) Level of Service Handbook.

Roadway Performance

Thornydale Road has an LOS D daily volume threshold of 16,730 vehicles per day (vpd) and Overton Road east of Thornydale Road has an LOS D daily volume threshold of 10,660 vpd based on FDOT criteria. Based on the recorded traffic volumes shown in Exhibit 3, the daily volumes on Thornydale Road exceed the LOS D daily threshold volumes. Overton Road operates below its LOS D daily volume thresholds.

Site Traffic Forecasting

The future traffic from the project is estimated using the trip rates contained in the Institute of Traffic Engineers' *Trip Generation Manual, 11th Edition* for the various land uses. Trip generation is the mathematical product of land use intensity (building square footage, number of units, etc.) and the trip generation rate. The result is the total number of one-way trips expected to be generated by the project. These trips represent the number of vehicles estimated to enter and leave the project site.

Trip Generation

Exhibit 5 provides the ITE average trip rates and trip generation for the proposed use during the average weekday. The exhibit shows the number of trips generated by the project for the three-time periods (weekday, weekday am peak hour, and weekday pm peak hour) at build out of the project.

Exhibit 5 Trip Generation

Trip Generation Rates			AM Peak		PM Peak		Weekday	
Land Use	1000 SF	ITE Code	In	Out	In	Out	In	Out
Mini-Warehouse	114.8	151	0.09		0.15		1.45	
			59%	41%	47%	53%	50%	50%

Trip Generation			AM Peak		PM Peak		Weekday	
Land Use	1000 SF	ITE Code	In	Out	In	Out	In	Out
Mini-Warehouse	114.8	151	10		17		166	
			6	4	8	9	83	83

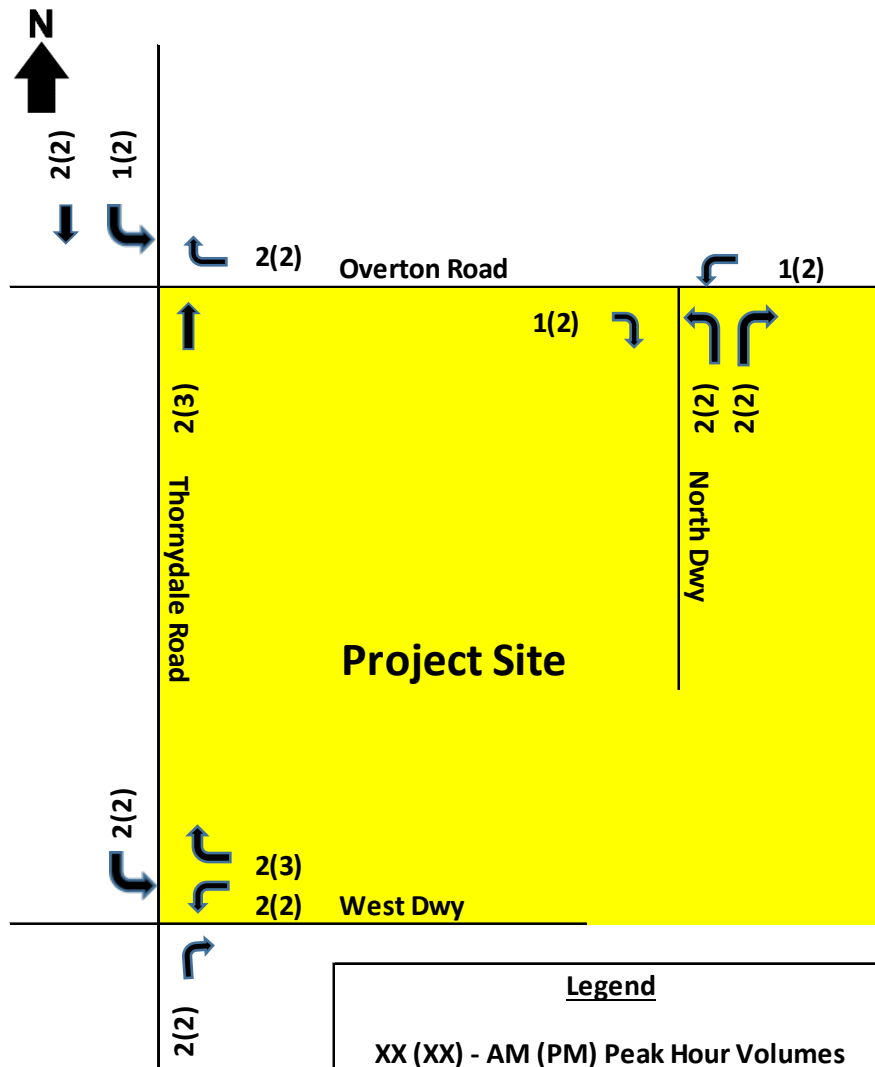
Pass-By Trips

No pass-by trips were assumed for the trip generation.

Trip Distribution and Assignment

Because the trip generation for this project is small, we distributed the trips equally out of the driveways as shown in Exhibit 6.

Exhibit 6 Trip Distribution



Site Traffic Forecasting

Exhibit 7 shows the year 2023 volumes on Thornydale Road and on Overton Road under “no project” and “with project” conditions. Background traffic was grown by 2%/year to estimate 2023 no project volumes and the site trips were added to these volumes estimate 2023 with project volumes.

Thornydale Road will continue to have daily volumes over its LOS D threshold in the future with or without the project. The daily volumes on Overton Road will not exceed its LOS D daily volume capacity with the project volumes through 2023.

Exhibit 7 2023 Roadway Daily Volumes

Street	Daily Capacity at LOS D*	2023 ADT No Project	Site Trips	2023 ADT With Project	Over LOS D Capacity (No Project)	Over LOS D Capacity (With Project)
Linda Vista to Overton	16,730	20,302	66	20,369	Yes	Yes
Overton to Hardy	16,730	17,641	66	17,707	Yes	Yes
Overton Road						
West of Thornydale	10,660	1,411	0	1,411	No	No
Thornydale Road to Shannon Road	10,660	8,126	33	8,159	No	No

*Generalized Annual Average Daily Volumes for Florida's Urbanized Areas, from 2020 FDOT Quality/Level of Service Handbook Tables.

Auxiliary Lane Warrants

We applied right turn lane warrants from the *Pima County Subdivision and Development Street Standards*. They are based on the daily volume of the street where a potential turn lane may be and the peak hour turning volumes. The warrant for turn lanes also considers the posted speed limit on the street from which the turn would originate. We did not review left turn warrants because left turns into the site will be from existing two-way left turn lanes on Thornydale Road and Overton Road.

The Pima County turn lane warrant criteria are shown in Exhibit 8.

As shown in the exhibit, for a roadway with a 40-mph speed limit, there would have to be at least ten right turning vehicles per hour to warrant a right turn lane. Both Thornydale Road and Overton Road are posted for 40 mph. Based on the site trip generation and the trip distribution, there would not be more than ten right turning vehicles during the peak hour at either driveway. Therefore, a turn lane is not warranted at either driveway.

Crash Analysis

Collision data for the project intersections and adjacent roadway segments were provided by the Arizona Department of Transportation. Recorded collision data from 2016 through 2020 are shown in a summary in Exhibit 9 and 10.

Crash rates over 1.0 crash per million entering-vehicles (MEV) for intersections, or per million vehicle-miles (MVM) for roadways usually indicate a need to review mitigating measures to reduce the rate.

The intersection of Thornydale/Overton is below the 1.0 MEV over the five-year period. There were eighteen crashes with the predominant crash type being "rear end" crash (14).

The Thornydale Road segment from Overton Road to Hardy Road had twenty crashes during the five-year period and a five-year crash rate of 1.29 MVM, although the single-year crash rates were lower than 1.00 MVM during the last three years. The other roadway segments had crash rates lower than 1.0 MVM during the five-year period. The predominant crash type for the Thornydale Road segments was "rear end."

Exhibit 8 Pima County Right Turn Warrants on Two-Lane Roads

A-2 RIGHT TURN LANE GUIDELINES FOR TWO-LANE ROADS⁹

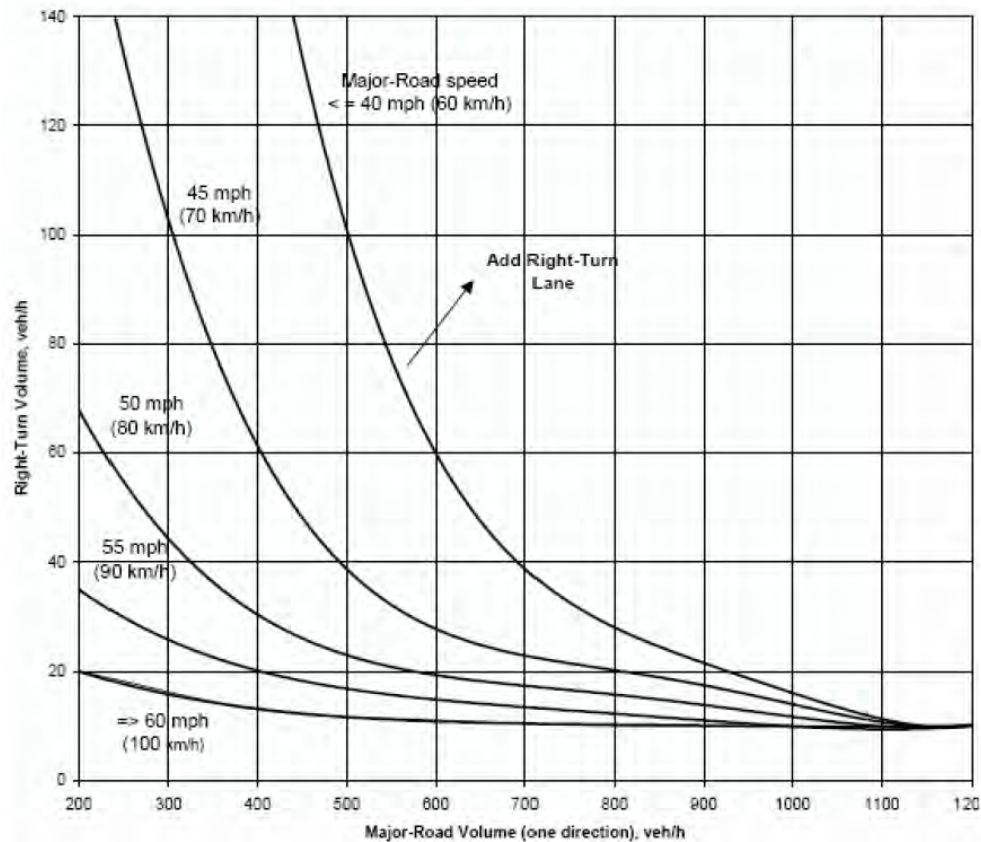


Exhibit 9 Collision History - Intersections

Thornydale/Overton							
Crash Type	2016	2017	2018	2019	2020	Total	%
Left Turn	1	2				3	17%
Rear End	3	3	2		6	14	78%
Sideswipe	1					1	6%
Total	5	5	2	0	6	18	
Crash Rate (per MVE)	0.60	0.60	0.24	0.00	0.72	0.43	
Severity						Total	%
Bodily Injury	1	2	2		2	7	39%
Property Damage	4	3			4	11	61%
Note: MVE = Million Vehicles Entering the intersection.							

Exhibit 10 Collision History - Roadways

Thornydale Road: Linda Vista to Overton Road							
Crash Type	2016	2017	2018	2019	2020	Total	%
Angle					1	1	6%
Rear End	4	2	3	3		12	75%
Head On		1				1	6%
Left Turn					1	1	6%
Rear to Rear	1					1	6%
Total	5	3	3	3	2	16	
Crash Rate (per MVM)	1.40	0.84	0.84	0.84	0.56	0.90	
Severity						Total	%
Bodily Injury	2		1	1		4	25%
Property Damage	3	3	2	2	2	12	75%

Thornydale Road: Overton Road to Hardy Road

Crash Type	2016	2017	2018	2019	2020	Total	%
Single Vehicle		1				1	5%
Rear End	10	2		3	2	17	85%
Sideswipe		1				1	5%
Other		1				1	5%
Total	10	5	0	3	2	20	
Crash Rate (per MVM)	3.23	1.62	0.00	0.97	0.65	1.29	
Severity						Total	%
Bodily Injury	1	2		1		4	20%
Property Damage	9	3		2	2	16	80%

Overton Road: Thornydale Road to Shannon Road

Crash Type	2016	2017	2018	2019	2020	Total	%
Single Vehicle	1					1	50%
Rear End			1			1	50%
Total	1	0	1	0	0	2	
Crash Rate (per MVM)	0.35	0.00	0.35	0.00	0.00	0.14	
Severity						Total	%
Bodily Injury			1			1	50%
Property Damage	1					1	50%

Note: MVM = Million Vehicle Miles

6. Conclusions and Recommendations

Conclusions

1. The project will generate an estimated 166 daily trips with 10 AM peak hour trips and 17 PM peak hour trips. These trips will be distributed to and from the project site at two driveways.
2. The driveway spacings and corner clearances will meet Pima County minimum spacing standards.
3. Based on criteria in the *Pima County Subdivision and Development Street Standards*, turn lanes are not warranted at the project driveways.

Recommendations

1. Construct the project driveways to Pima County standards, with one ingress and one egress lane.
2. Remove or trim vegetation at the Overton driveway location to ensure that there is acceptable sight distance to and from the project entrance.
3. Discuss with Pima County whether the project driveway on Thornydale Road should be restricted to rights in and out and left out due to the proximity of the Thornydale Road northbound left turn lane at Overton Road.
4. Roadway and subdivision design should conform to current Pima County standards.
5. All new traffic signs and markings must comply with the current *Manual on Uniform Traffic Control Devices* and local requirements.

Appendix

- Site Plan



SELF STORAGE
SEC THORNYDALE RD & OVERTON RD
TUCSON, AZ
DATE: 06-03-2022 (PRELIMINARY)

© 2019 RKAA
ARCHITECTS, INC. ALL
RIGHTS RESERVED.
THIS DRAWING IS AN
INSTRUMENT OF
SERVICE. IT IS THE
PROPERTY OF RKAA
ARCHITECTS, INC. AND
MAY NOT BE
DUPLICATED, USED OR
DISCLOSED WITHOUT
WRITTEN PERMISSION
OF THE ARCHITECT.

SP-1
SITE PLAN

RKAA# 22103.50



Appendix D

PCRWRD Type I Sewer Response

JACKSON JENKINS
DIRECTOR



PH: (520) 724-6500
FAX: (520) 724-9635

May 5, 2022

Ryan Stucki
EEC
555 E. River Road, Suite 301
Tucson, AZ 85704

Sewerage Capacity Investigation No. P22WC00135 Type I

**RE: Thornydale Self Storage, Parcel 22502027M
Estimated Flow 880 gpd (ADWF)**

Greetings:

The above referenced project is tributary to the Tres Rios Water Reclamation Facility via the Canada del Oro Interceptor.

Capacity is currently available for a project this size in the public sewer G-84-024, downstream from manhole 4201-16.

This letter is not a reservation or commitment of treatment or conveyance capacity for this project. It is not an approval of point and method of connection. It is an analysis of the system as of this date. Allocation of capacity is made by the Type III Capacity Response.

If further information is needed, please feel free to contact us at (520) 724-6488.

Reviewed by: Mirela Hromatka, Planner Sr.

Appendix E

Conceptual Elevations





Appendix F

Parking Demand Letter

June 13, 2022
Thornsdale/Overton Mini-Storage
Parking Demand Letter

Mr. Zachary Channing
ZDC Properties
18381 Long Lake Drive
Boca Raton, FL 33496

Dear Mr. Channing:

This letter is provided to support your rezoning application for the self-storage project on the southeast corner of Thornsdale Road/Overton Road in unincorporated Pima County. This letter provides justification for a parking supply of ten marked spots for a +/- 115,000 square foot self-storage project (the "Project").

1. M Esparza Engineering prepared a traffic memorandum for this project and found that the trip generation for the project would be eighteen trips during the PM peak hour with eight inbound trips and nine outbound trips during this highest peak hour. This indicates that even in the highest trip generation time frame, only eight customers will be visiting the site over the peak hour thus indicating a low parking demand. In addition, most of the current customers will be using the loading zone spaces in visiting their storage units.
2. The Institute of Transportation Engineers' current *Parking Generation Manual* (3rd Edition) includes parking demand estimates for the land use "Mini-Warehouse". The facility is expected to have about 750 storage units. The rate is 1.36 parking demand per 100 storage units, which would have a parking demand of 10 spaces.
3. Customers typically park near the entrances to their storage unit or in the loading zone for an interior self-storage facility and not necessarily in the marked parking spaces. An example of this is attached. The aerial photo is from the National Self Storage facility at 12071 North Thornsdale Road in Dove Mountain.
4. The following guidance is from M. Anne Ballard, the President of Marketing, Training and Development Services of United Storage Group, an industry consultant on storage facilities, answering a question of how many storage units and parking spaces are required for a +/- 115,000 square foot storage facility: "You should get between 600 and 650 spaces out of that, and it seems you have more than adequate parking. With that many units you would likely have 3-5 customers on site per day during the week and all-day Saturday maybe 10-15".¹
5. The City of Mesa Arizona's Code of Ordinances, Title 11 Zoning Ordinance, Article 4 Development Regulations Chapter 32 On-Site Parking, Loading, And Circulation provides the following:

¹ E-mail from M. Anne Ballard (United Storage Group) to Zachary Channing (ZDC Properties), June 6, 2022.

11-32-3: PARKING SPACES REQUIRED


For independent industrial buildings and uses, sub-category "mini-storage", the requirement is 4 spaces plus 2 for manager's quarters.

There is no ratio provided in this code requirement.

Based on the above, the 10 marked spaces provided for the Project plus the space to park 2-4 within the loading zone should be more than adequate for the proposed land use.

Please feel free to contact me if you have any questions or comments.

Sincerely,



Marcos Esparza, P.E.
Principal

M Esparza Engineering, LLC

2934 W. Salvia Drive
Tucson, Arizona 85745
Office: 520-207-3358
Mobile: 520-419-5909
E-mail: mue-cla@cox.net



12071 N Thornydale Rd, Marana

12071 N Thornydale Rd

Building

Directions

Save

Nearby

Send to phone

Share

12071 N Thornydale Rd, Marana, AZ 85658

Suggest an edit on 12071 N Thornydale Rd

Add a missing place

Add your business

Add a label

Photos

At this location

National Self Storage - Dove Mountain

4.8 ★★★★★ (96)

Self-storage facility · 12071 N Thornydale Rd



Appendix G

Metro Water Letter



In Email

May 26, 2022

Ryan Stucki, P.E., LEED AP
EEC
555 E. River Road, #301
Tucson, Arizona 85704

**Re: SEC of Thornydale Road and Overton Road
(APN 225-02-027M)
CAP22-05**

Dear Mr. Stucki,

The Metropolitan Domestic Water Improvement District (MDWID) is certified to provide water to the above referenced property and is designated as having a 100-year assured water supply.

Any onsite or offsite requirements deemed necessary will be determined at the time of improvement plan submittal or whenever application for water service is received and will be the financial responsibility of the owner or those developing the property. Pipe sizing and system augmentation will be determined per District design criteria at the time of plan submittal.

If an improvement plan has not been submitted within 2 (two) years after the date of this letter, a reevaluation and reissuance of this will-serve letter will be necessary.

Please let me know if you have any questions or concerns at 575-8100.

Sincerely,

A blue ink handwritten signature, which appears to be "Timothy Dinkel", is written over a blue circular stamp or seal.

Timothy Dinkel, P.E.
Engineering Manager

Enclosure

c: Project File